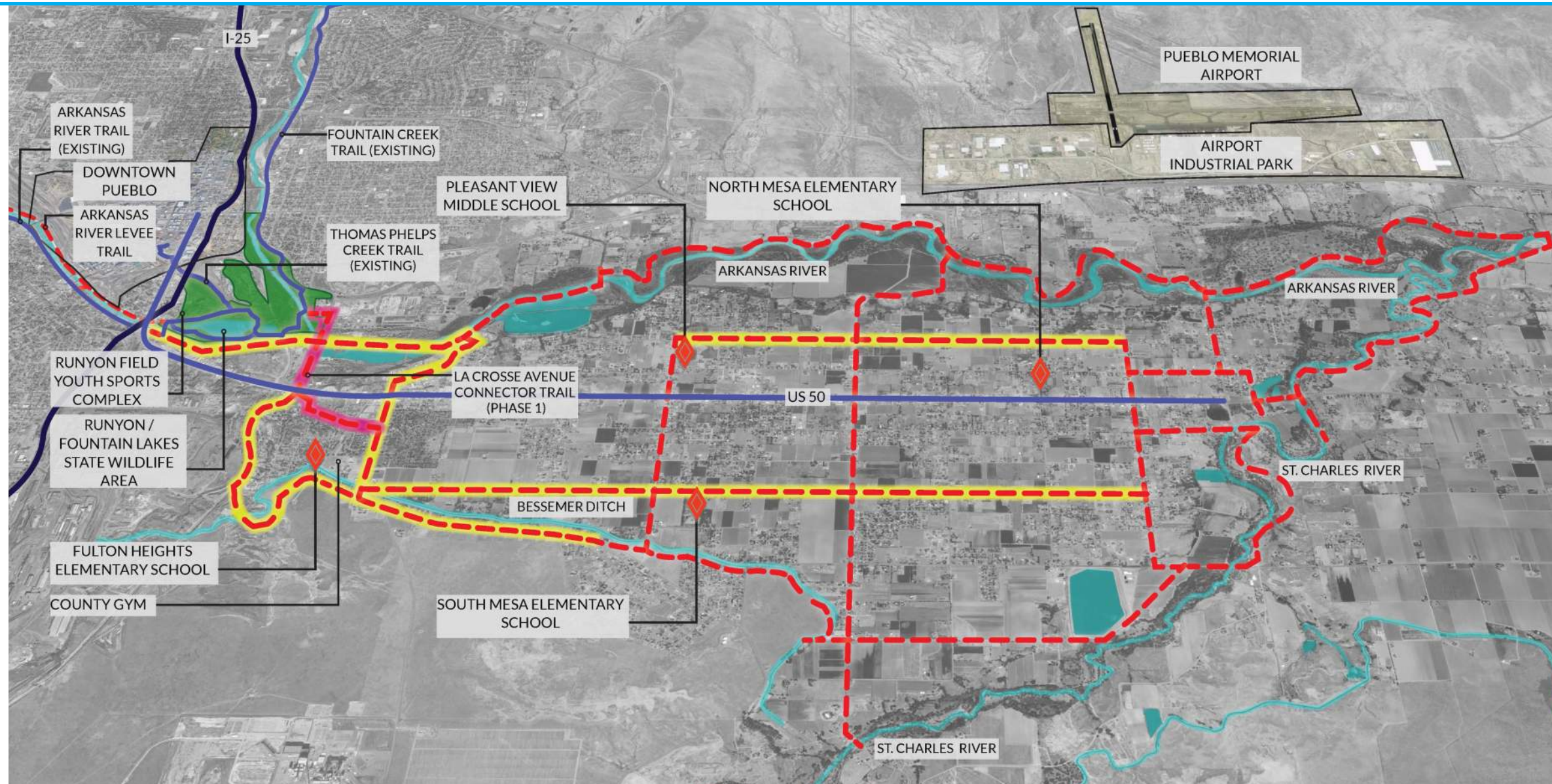


Pueblo River Trails Extension Master Plan 2020 Update



FINAL for CDOT/FHWA REVIEW

March 11, 2021

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Table of Contents

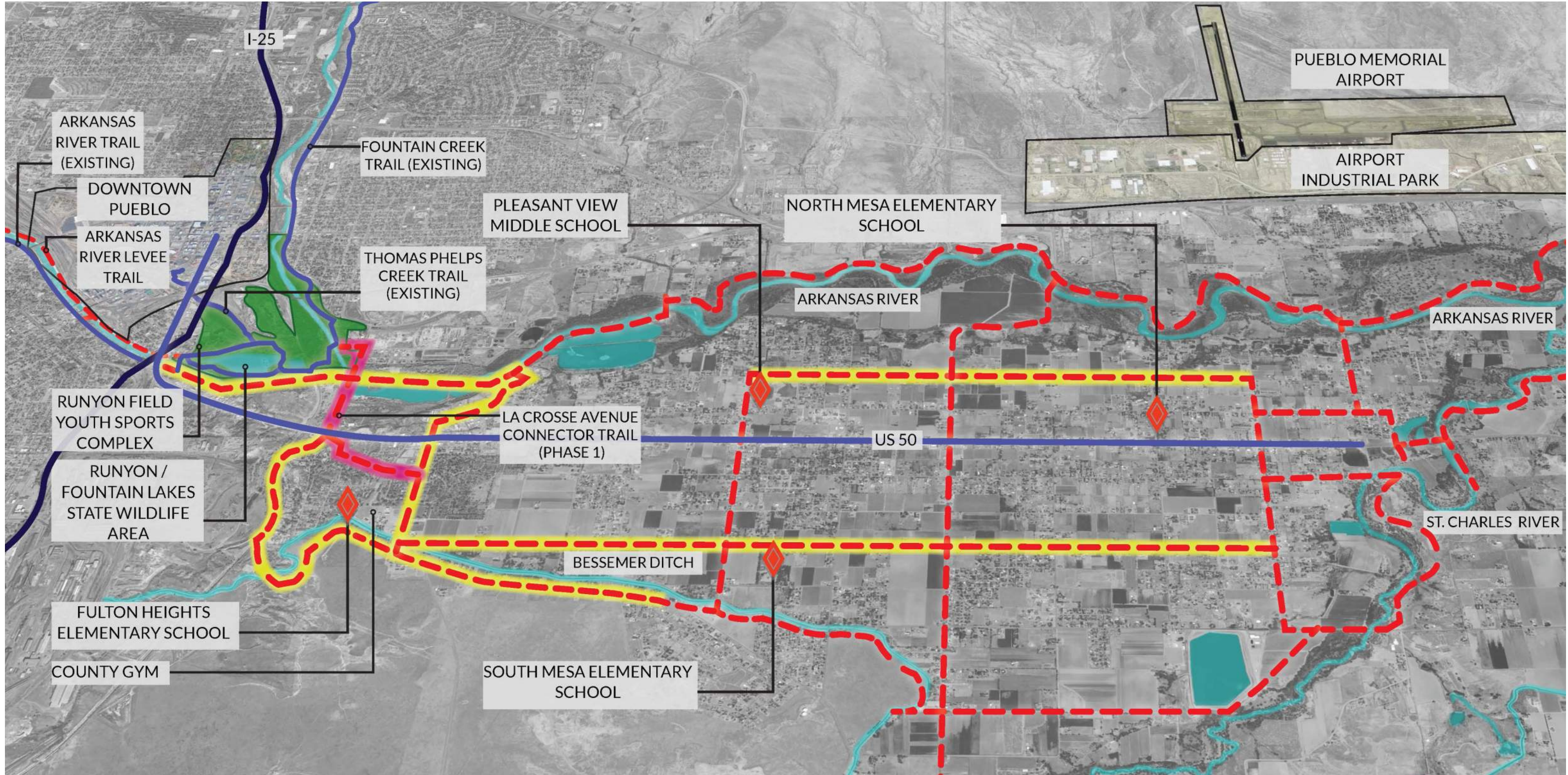
Context / Overview Plan	2
Overall Master Plan	3
Priority Trail Segments	
La Crosse Avenue Connector Trail	4
Arkansas River Trail	6
Salt Creek Trail	12
Aspen Creek Trail	14
Bessemer Ditch	16
County Farm Road	18
Everett Road	20
Public Engagement	23
Trail Management	25
Appendix A - Technical Appendix	29

Under separate cover:

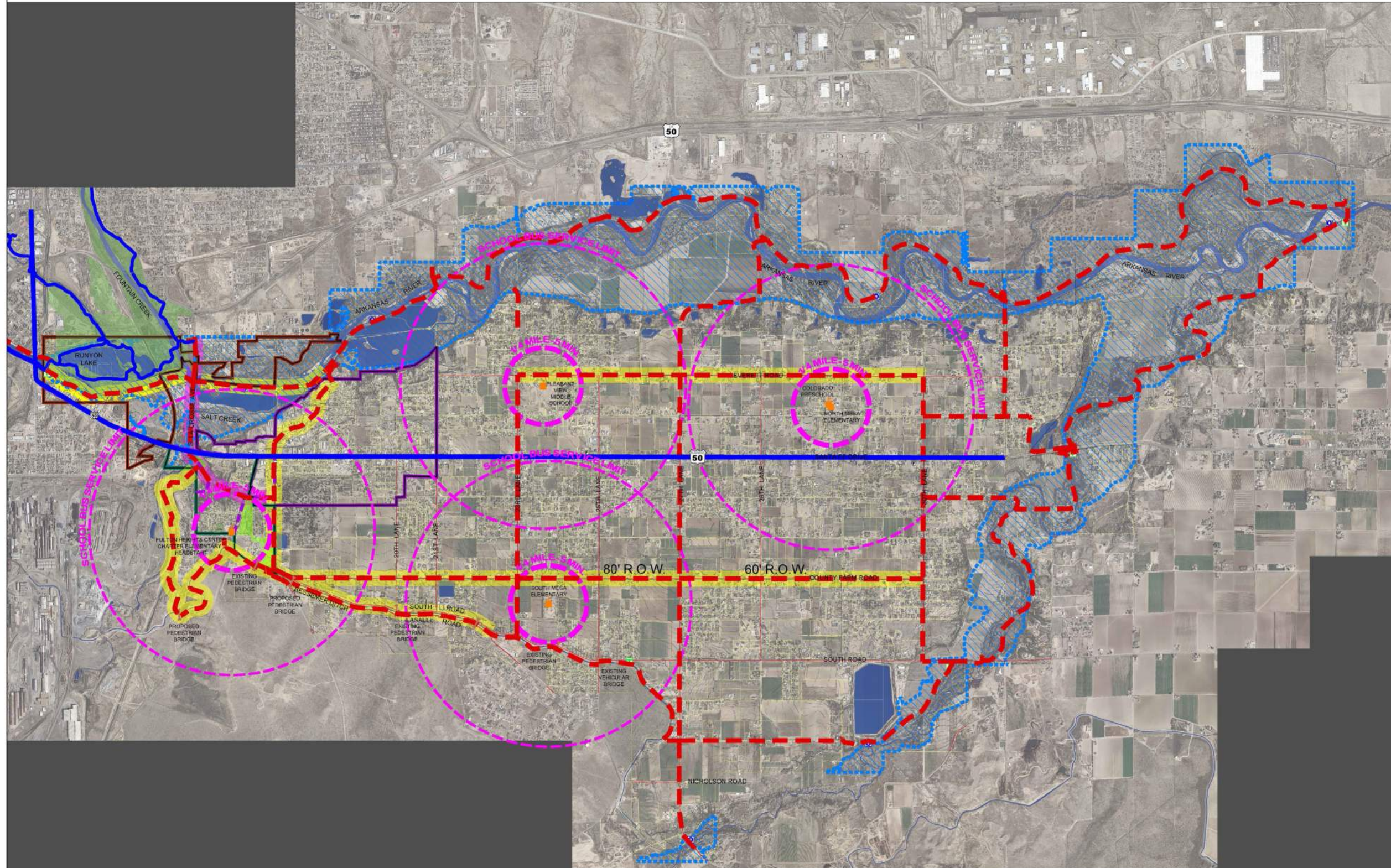
Appendix B - Master Site GIS Map Book

Appendix C - 1998 Trail Extension Master Plan

Context / Overview Plan



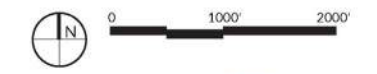
Overall Master Plan



TRAIL ALTERNATIVES PUEBLO RIVER TRAIL EXTENSION MASTER PLAN PUEBLO COUNTY, CO

LEGEND

- EXISTING TRAIL / ON-STREET BIKE LANES
- HIGH PRIORITY TRAIL ALTERNATIVE
- LA CROSSE AVENUE CONNECTOR TRAIL (PROPOSED PHASE 1)
- EXISTING ELECTRIC LINE
- EXISTING BUS ROUTE
- PROPOSED TRAIL
- EXISTING RAILROAD
- EXISTING PARCEL BOUNDARY
- MUNICIPAL BOUNDARY - PUEBLO
- MUNICIPAL BOUNDARY - BLENDE
- MUNICIPAL BOUNDARY - SALT CREEK
- - - 100-YEAR FLOODPLAIN
- EXISTING PARK
- F EXISTING FIRE STATION
- B EXISTING BUS STOP
- S EXISTING SCHOOL
- X PROPOSED TRAIL CROSSING
- X PROPOSED OVERLOOK
- P PROPOSED PARKING LOCATION
- T PROPOSED TRAILHEAD



La Crosse Avenue (SH 227) Connector Trail

Context

The La Crosse Avenue Connector Trail provides a direct connection via On-Street Bicycle Lanes, between the Existing Pueblo County Fountain Creek Trailhead on La Crosse Avenue, and the proposed Mesa Community Center site, on Aspen Street. Because the Existing Fountain Creek Trailhead is currently linked to the greater Pueblo Regional Trail network, completion of this critical trail link will connect the St. Charles Mesa with the Runyon/ Fountain Lakes State Wildlife Area, Fountain Creek Greenway Corridor, Runyon Field Youth Sports Activity Center, Downtown Pueblo and the Historic Arkansas Riverwalk of Pueblo (HARP). This trail connection reaches as far as Lake Pueblo State Park, to the west and ultimately, to the Pueblo West trail system to the north.

Role

The La Crosse Avenue Connector Trail's role in the regional trail network is to provide a near-term, cost-effective, implementable connection between the St. Charles Mesa, Downtown Pueblo and the new investment in the Runyon Activity Area. It adds multi-modal (bike) capabilities to the La Crosse Avenue (SH 227) corridor, by expanding utilization of the only grade-separated crossing (underpass) of the Santa Fe Highway (US 50C) that serves the Mesa, to provide a safe north/south route that minimizes highway crossing conflicts.

Alignment

The La Crosse Avenue Connector Trail Segment starts at the existing Pueblo County Fountain Creek trailhead, on the east bank of Fountain Creek, near the confluence of the creek and the Arkansas River. It connects westward with the existing trail system of the Runyon/ Fountain Lakes State Wildlife Area via an existing multi-use pedestrian bridge over Fountain Creek. The State Wildlife Area Trail system connects further to the west via the existing Arkansas River Regional Trail, the proposed Arkansas River Levee Trail and to

Downtown Pueblo via the existing Thomas Phelps Creek Trail, to HARP. The existing trailhead also connects with the Fountain Creek Regional Trail Corridor, extending northward.

The existing concrete trail (10' width) along the south edge of the trailhead parking lot is proposed to be extended eastward to the La Crosse Ave. (SH 227) ROW and existing roadway pavement with the addition of a culvert over the roadway drainage swale.

Existing street pavement is proposed to be roto milled to a depth of 2" and repaved with a new surface layer of asphalt pavement for the entire width of the roadway pavement, starting immediately north of the trail head entry drive, extending south to the existing Arkansas River Bridge. The newly repaved roadway is proposed to be restriped to provide two (2) travel lanes of 12' each, with a 10' designated on-street bike lane on the west side.

Because the Arkansas River Bridge is more narrow than the approach pavement, a 750' transition is proposed where bicyclists and vehicular traffic will merge as the on-street bike lane tapers to merge with the vehicular travel lane. The shared lane configuration, known as a 'Sharrow' will be indicated with on-pavement Bicycle markings, and regulatory signage alerting vehicles and bicyclists of the merge, and shared road condition.

South of the Arkansas River Bridge, another 750' pavement transition will be provided, as the designated on-street bike lane transitions back to the full 10' lane width. Roto-milling and asphalt repaving is proposed to continue southward on La Crosse Ave (SH227). At the existing Salt Creek Bridge, the On-street bike lane configuration will change, with the addition of concrete barriers (54" Ht.) on each side of the 10' Trail. The concrete

barrier between the bike lane and traffic is to provide a protected lane from adjacent traffic maneuvering through the La Crosse Ave. (SH227) Santa Fe (US 50C) interchange. Concrete barrier between the trail and Salt Creek is provided to prevent cyclists from the steep drop from the trail/roadway surface down to Salt Creek, below. The 'barrier' section is proposed with concrete surfacing, to provide a more durable surface, requiring less regular maintenance/ replacement between the concrete barriers. The concrete trail and 'barrier' protection are proposed to extend to the south side of the Santa Fe (US 50C) underpass.

On the south side of the underpass, milling and asphalt repaving is proposed for the entire paved roadway section, following Roselawn Avenue alignment to Aspen Street intersection.

Restriping in this segment is proposed as two (2) 4' width designated on-street bike lanes, one on each side of each travel lane. It is important to note that the 4' dimension of these bike lanes does NOT include the 2' curb and gutter pan, which in effect allows a full 6' width between the lane stripe and vertical concrete curb.

Trail Components

This Trail Segment is approximately one and one third miles (1.35 mi.) in length and is proposed of 10' width, asphalt, On-Street Trail and 10' width concrete trail with protective concrete barriers, north of the Santa Fe (US 50C) Underpass. The Arkansas River Bridge and transitions are On-Street shared lane "sharrows". The entire trail route north of the Salt Creek bridge is located in the floodplain of Salt Creek and the Arkansas River. South of the Santa Fe underpass, the Trail is comprised of two (2), asphalt on-street designated bike lanes, 4' width each.

Property Ownership

Public ROW: 7,156 LF = 100%
Private Ownership: None

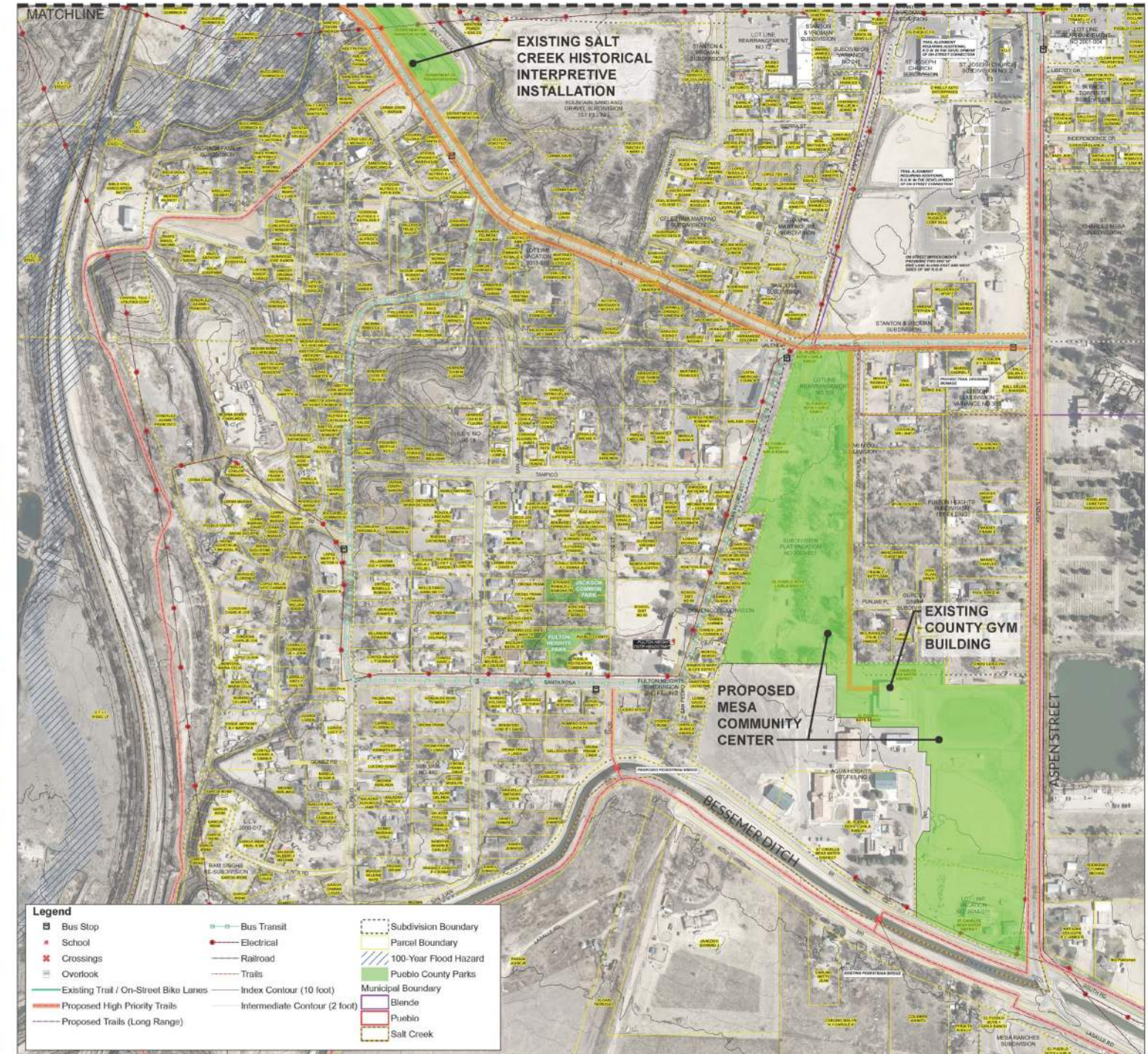
Estimated Cost

\$1.3 M

Primary Issues

- La Crosse Ave. (SH227) Interchange and Underpass Improvements
- Arkansas River Bridge Transitions

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
LA CROSSE AVENUE CONNECTOR PLAN (Alt 2)	
REACH No. 1 (Trailhead Parking to Bridge Transition)	\$96,229
REACH No. 2 (Bridge Section & Transitions)	\$10,337
REACH No. 3 (Bridge Transition to Guardrail Section)	\$64,463
REACH No. 4 (Guardrail Section)	\$516,010
REACH No. 5 (Guardrail Section to Aspen Street)	\$187,517
TOTAL	\$874,556
Design Fee, Testing, Traffic & Erosion Control	\$262,367
Contingency - 20%	\$174,911
TOTAL	\$1,311,833



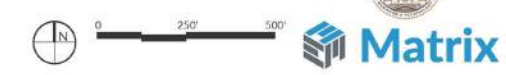
Legend

Bus Stop	Bus Transit	Subdivision Boundary
School	Electrical	Parcel Boundary
Crossings	Railroad	100-Year Flood Hazard
Overlook	Trails	Pueblo County Parks
Existing Trail / On-Street Bike Lanes	Index Contour (10 foot)	Municipal Boundary
Proposed High Priority Trails	Intermediate Contour (2 foot)	Blende
Proposed Trails (Long Range)		Pueblo
		Salt Creek

LaCrosse Avenue Connector Trail
Pueblo River Trails Extension Master Plan

October 1, 2020

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Arkansas River Trail

Context

The Arkansas River Trail segment seems to be a logical starting point for the Pueblo River Trails Extension Master Plan. However, there are considerable development challenges that exist. A series of alternatives were explored to better understand those challenges and how they might be overcome prior to implementation of this trail segment. Physically, a trail connection can be made to work through this area, but very costly environmental remediation, land or public easement purchases or expensive pedestrian bridge improvements make implementation of this trail segment infeasible for the foreseeable future.

Role

The Arkansas River Trail Extension's role in the regional trail network is to provide expanded recreational riverfront access and provide a sufficient eastward extension of the trail network to link up with logical trail corridor connection points that lead south onto the St. Charles Mesa.

Alignment

The Arkansas River Trail Segment starts at the existing Moffat Street trailhead, on the south bank of the river, where the existing Arkansas River Trail crosses the river to the north bank, connecting with the existing trail system of the Runyon/ Fountain Lakes State Wildlife Area. Because the Arkansas River Regional Trail is currently located on the south bank of the river upstream to the west, and is intended to connect to southward linkages further east, the most direct connection is extending the trail on the south bank of the river. The trail alignment on south bank crosses private property, but avoids crossing Fountain Creek, which has its confluence with the river along this reach, west of La Crosse Avenue (SH 227). A public access easement would be necessary for the trail to cross private property.

A pedestrian bridge connection from the proposed trail on the south bank to the north side of the river is proposed to further improve access and circulation to and from Runyon Lake, and to provide a river trail connection to the existing Pueblo County Trailhead and parking area on the east side of Fountain Creek, with access from La Crosse Avenue (SH 227).

On the south bank, continuing the eastward alignment of the trail, the route encounters the Burlington Northern Santa Fe (BNSF) Railroad tracks, providing north/south freight rail service through Colorado. On the south bank, the railroad bridge over the Arkansas River has several approach segments that elevate the track above grade before reaching the riverbank. A trail underpass is proposed to provide a safe crossing solution and avoid potential pedestrian/ railroad conflicts. A public access easement would be necessary for the trail to cross the BNSF Railroad ROW.

East of the BNSF Tracks, after dipping down under the railroad tracks, the proposed alignment must rapidly climb up to existing grade, across private property, before steeply dropping down to pass under the La Crosse Avenue (SH227) bridge over the Arkansas River. A public access easement would be necessary for the trail to cross private property between the BNSF tracks and the La Crosse Ave (SH227) public ROW.

East of the La Crosse Avenue (SH 227) ROW, the Arkansas River Trail alignment continues east along the river, following an existing asphalt service road across private property to its terminus, the Blende Sanitary Sewer District access road from Aspen Circle. A public access easement would be necessary for the trail to cross private property.

Trail Components

The Arkansas River Trail Extension's role in the This Trail Segment is approximately two miles (2 mi.) in length and is proposed exclusively of 10' width, concrete, Off-Street Trail. The entire trail route is located in the floodplain of the Arkansas River. One (10'width) pedestrian bridge is proposed over the Arkansas River at three (3) alternative locations. Extensive grading, drainage and site preparation will be required. An extensive ramped access trail with retaining walls is proposed at its eastern terminus to connect with Aspen Circle.

Property Ownership

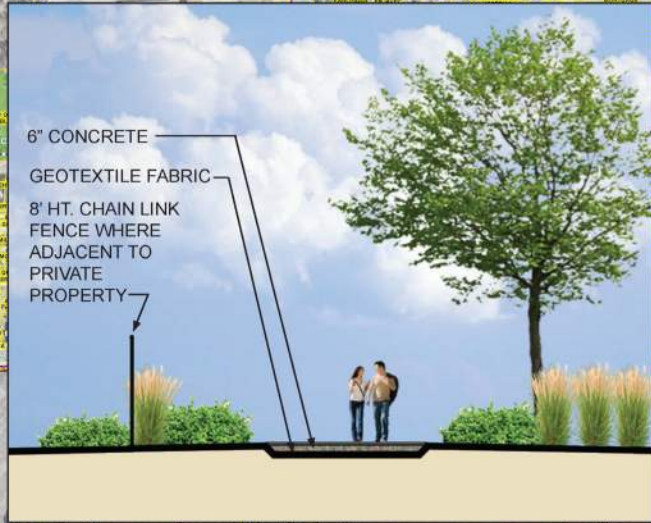
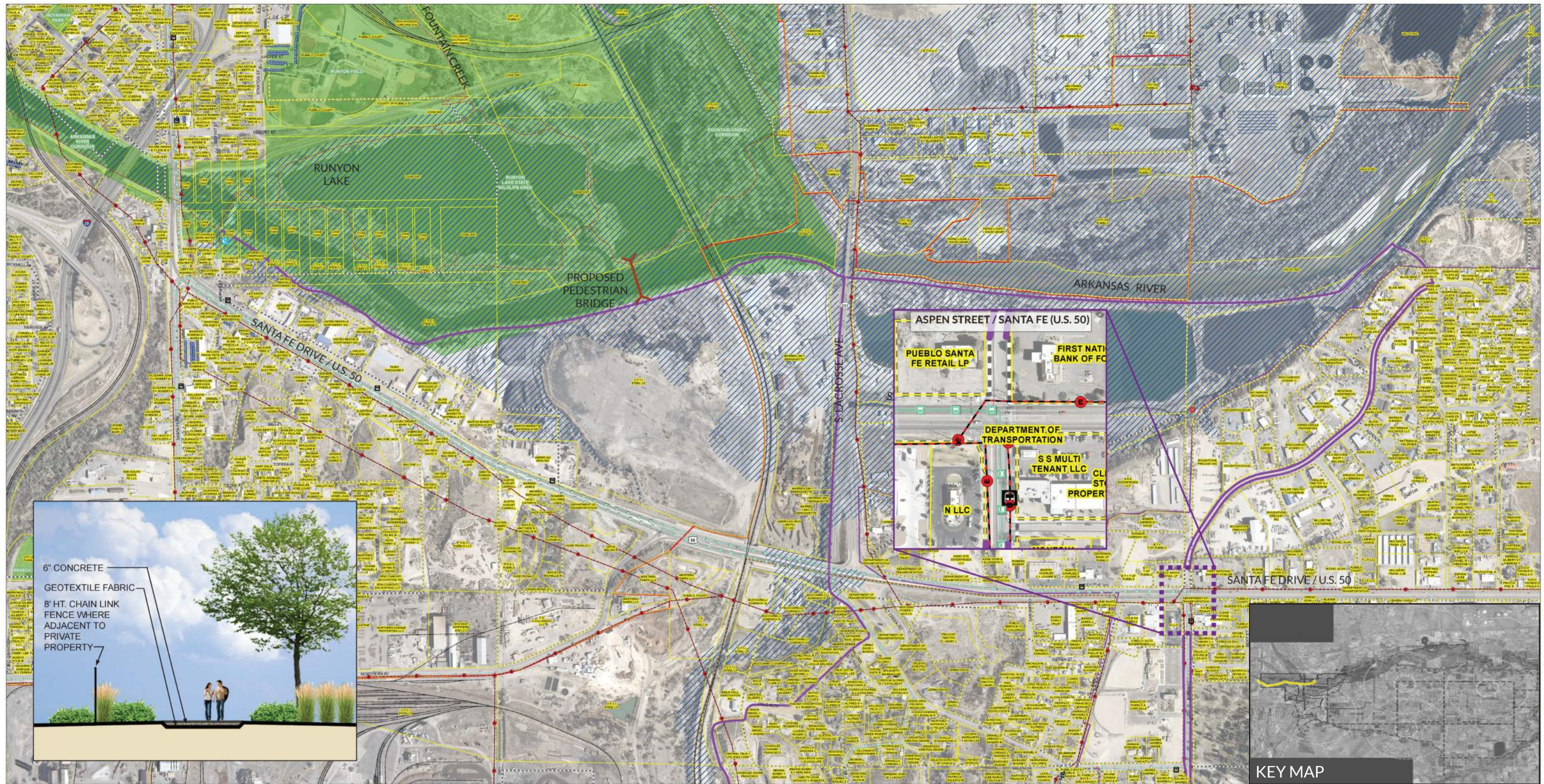
Public ROW:	820 LF = 8%
Private Ownership:	
EVRAZ Steel:	5,005 LF = 50%
Warren, Jim & Bernadette:	897 LF = 9%
Phillips, Thomas G:	2,566 LF = 25%
Continental:	1,338 LF = 8%
Blende Sanitation District:	107 LF = <1%

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
ARKANSAS RIVER TRAIL 250' BRIDGE ALTERNATIVE (Alt 1/3)	
REACH No. 1 (Moffat Street Trailhead to LaCrosse Ave.)	\$1,151,507
REACH No. 2 (LaCrosse Ave. to Aspen Court)	\$478,361
REACH No. 3 (Aspen Court Trailhead Section)	\$832,644
250' PEDESTRIAN BRIDGE COST	\$1,317,168
TOTAL	\$3,779,680
Design Fee, Testing, Traffic & Erosion Control	\$1,133,904
Contingency - 20%	\$755,936
TOTAL	\$5,669,520
ARKANSAS RIVER TRAIL 150' BRIDGE ALTERNATIVE (Alt 2)	
REACH No. 1 (Moffat Street Trailhead to LaCrosse Ave.)	\$1,151,507
REACH No. 2 (LaCrosse Ave. to Aspen Court)	\$478,361
REACH No. 3 (Aspen Court Trailhead Section)	\$832,644
150' PEDESTRIAN BRIDGE COST	\$407,635
SUBTOTAL	\$2,870,147
Design Fee, Testing, Traffic & Erosion Control	\$861,044
Contingency - 20%	\$574,029
TOTAL	\$4,305,220

Primary Issues

- EVRAZ Environmental Deed Restriction
- Private Property Public Access Easements
- Bridge Crossing Alternatives



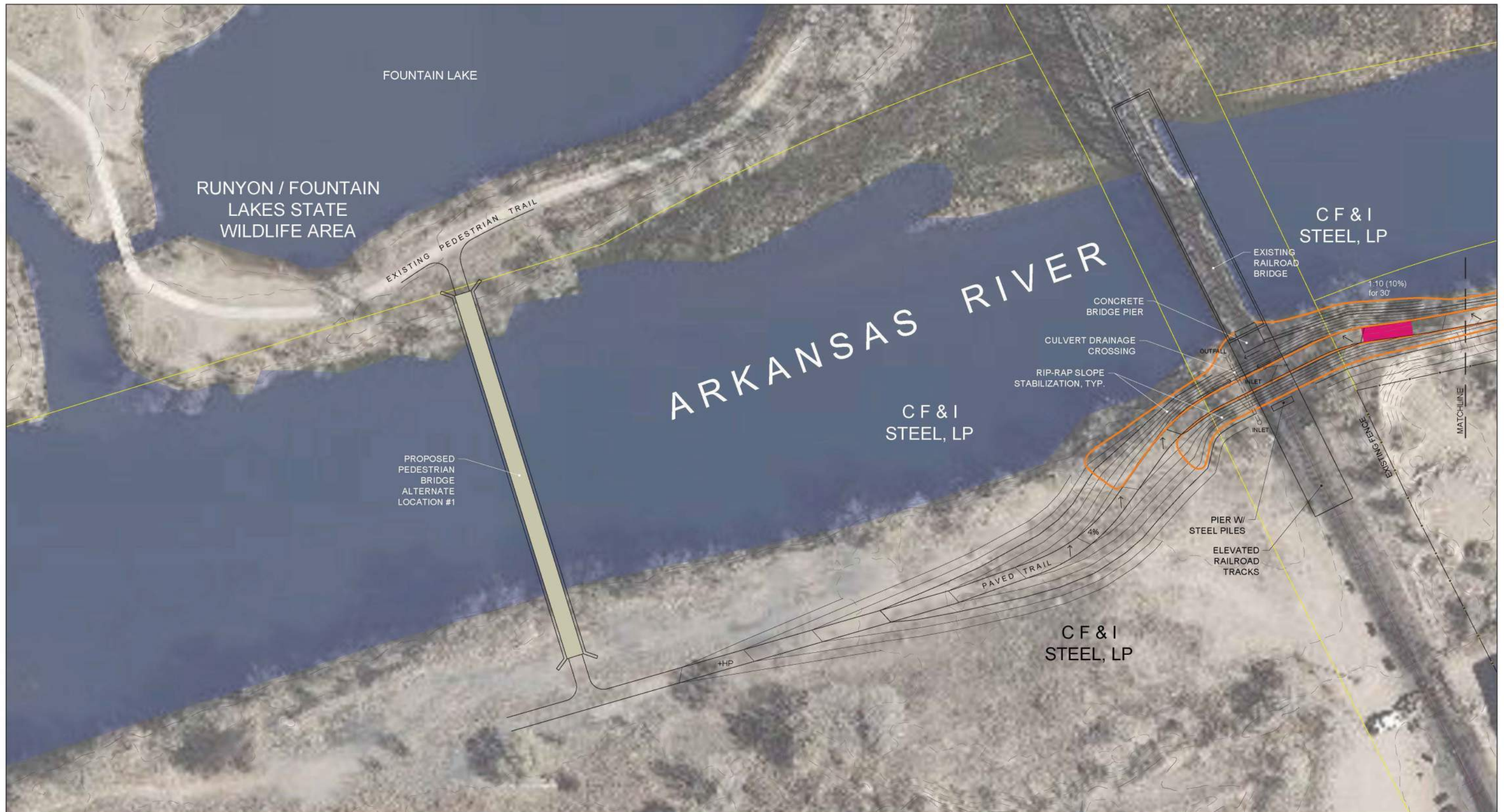
Arkansas River Trail

Pueblo River Trails Extension Master Plan

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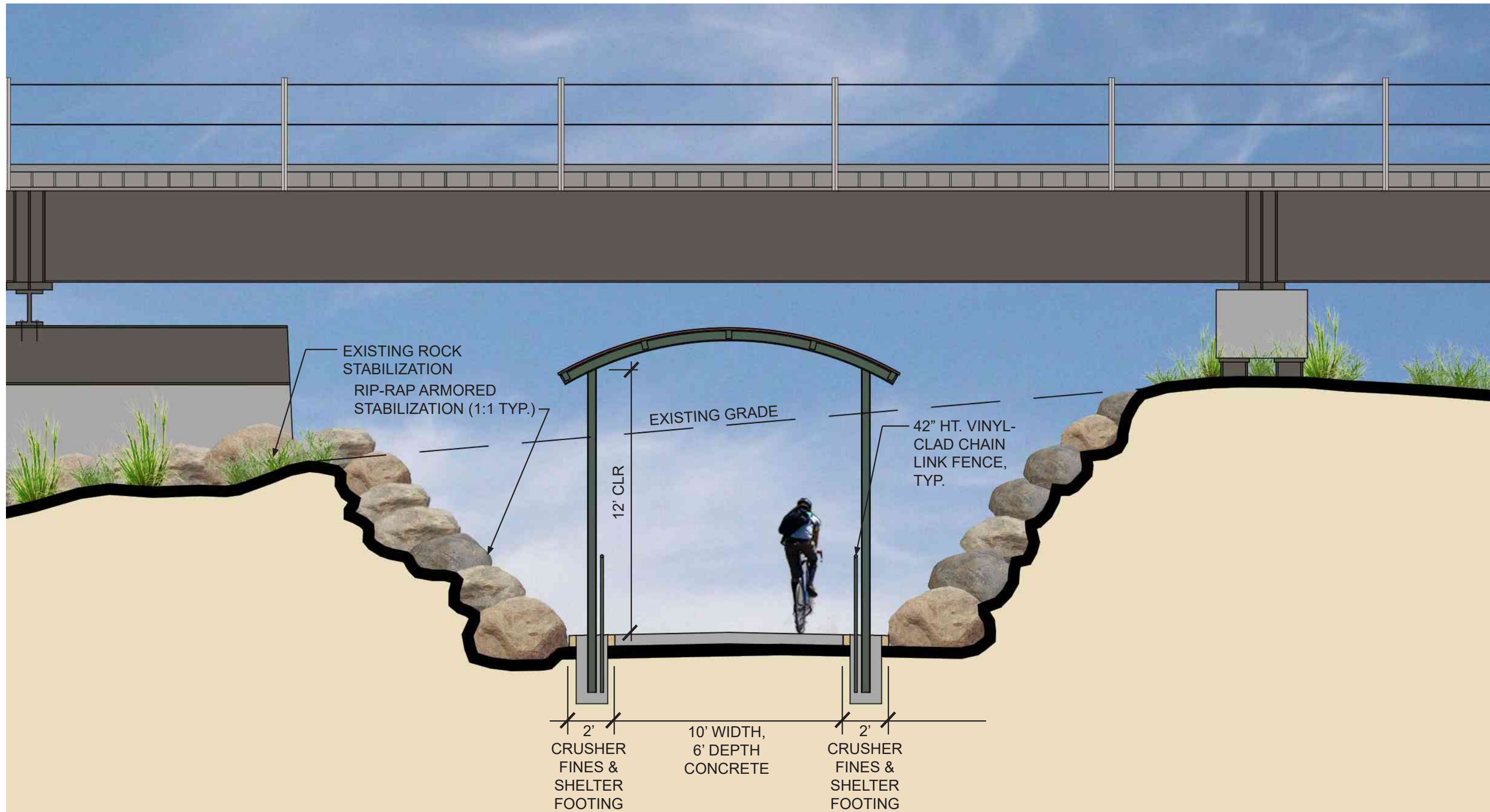
Arkansas River Trail - Bridge (Alt 1) & Railroad Underpass Plan

Pueblo River Trails Extension Master Plan

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Railroad Underpass Section

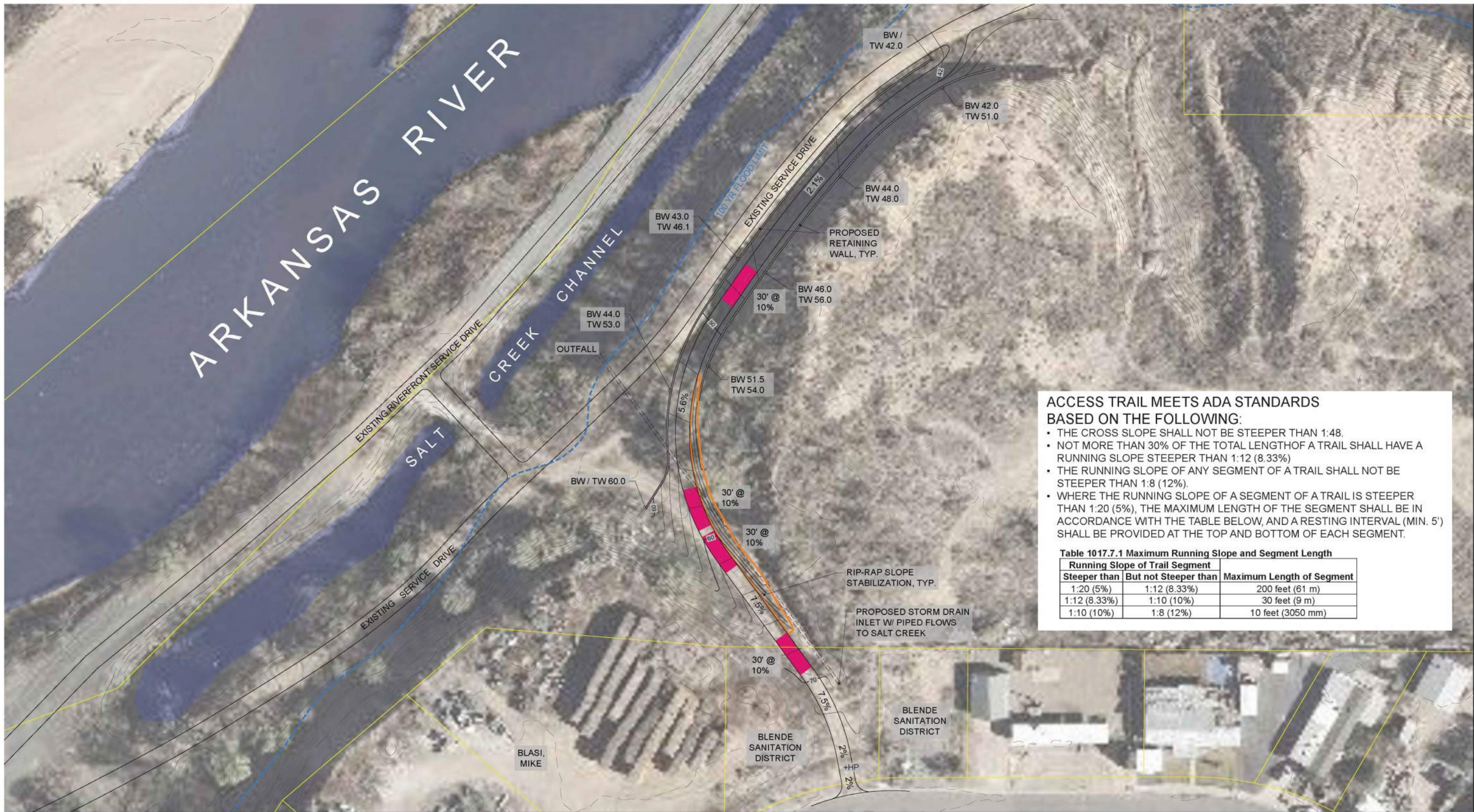
Pueblo River Trails Extension Master Plan



October 1, 2020

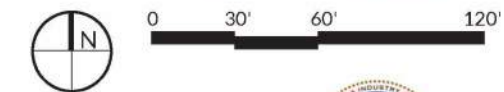
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Arkansas River Trail - Blende Sanitation District Trail Access

Pueblo River Trails Extension Master Plan



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Salt Creek Trail

Context

The Salt Creek Trail Corridor rises from the shady riverside Arkansas River Trail to the southern edge of the arid St Charles Mesa, passing through industrial, Salt Creek-side and historic residential neighborhood landscapes, to emerge along an undeveloped rail corridor with views to the west of the historic Colorado Fuel and Iron (CF&I, now EVRAZ) Steel Plant.

Role

The Salt Creek Trail's role in the regional trail network is to provide a north/ south connection of the trail network from the Arkansas River Trail to connect with trail connections that extend eastward across the St. Charles Mesa. The Salt Creek Trail corridor is the single opportunity for a grade separated trail crossing of Santa Fe (US 50C) in the western St. Charles Mesa.

Alignment

The Salt Creek Trail Segment starts at the Arkansas River Trail, on the south bank of the river, at the trail underpass of the La Crosse Avenue (SH227) bridge. The trail alignment follows the existing asphalt service road access ascending from below the bridge along the west side of La Crosse within the public road ROW, to the entry drive and parking lot of American Iron and Metal, Inc. To minimize potential entry drive conflicts, the trail is proposed to connect with a proposed concrete Off-Street Trail within the ROW, and adjacent to the course of Salt Creek that extends to the south side of the Santa Fe (US 50C) underpass.

In order to provide sufficient width between the La Crosse travel lanes and the precipitous slope dropping down to course of Salt Creek, approximately 30' below, additional pavement and restriping of the travel lanes (moving them approximately 20' to the east) would be necessary.

The existing highway guardrail at the top of the drop-off is proposed to remain/ be

replaced, and a second guardrail between the trail and travel lanes is also proposed. Sufficient ROW and roadway space exists under the Santa Fe (US 50C) underpass for the necessary lane shifts, with the potential to ease the 'reverse curve' condition connecting with Roselawn Avenue. Potential lane realignment farther north, adjacent to the northbound/southbound ramp crossings, requires further coordination with CDOT design engineers.

South of the Santa Fe (US 50C) underpass, On-Street Bike Route (Sharrow) are proposed from the Roselawn Road intersection (on the west side) to Palo Alto, the first public street on the right. Again, additional paving on the east side of La Crosse Avenue through this area, with restriping to shift lanes east, could provide additional width for Sharrow users and improve traffic safety.

Following Palo Alto into the Salt Creek Neighborhood the proposed Sharrow continues over the existing Salt Creek crossing, past the central park area to the western terminus of the neighborhood. A proposed Off-Street Trail connection to the west is proposed at this location to continue the Off-Street Trail to the south, connecting with the proposed Bessemer Ditch Trail.

A public access easement across private property will be required.

Trail Components

This Trail Segment is approximately one and one-half miles (1.5 mi.) in total length. It is comprised of just under one mile (1 mi.) of 10' width, concrete Off-Street Trail (59%), and just over one-half mile (0.5 mi.) of On-Street Bike Route (Sharrow) (41%). Extensive grading, drainage and site preparation will be required. La Crosse Avenue (SH 227) pavement additions and restriping could potentially escalate estimated costs.

Property Ownership

Public ROW: 4,377 LF = 50%

Private Ownership:

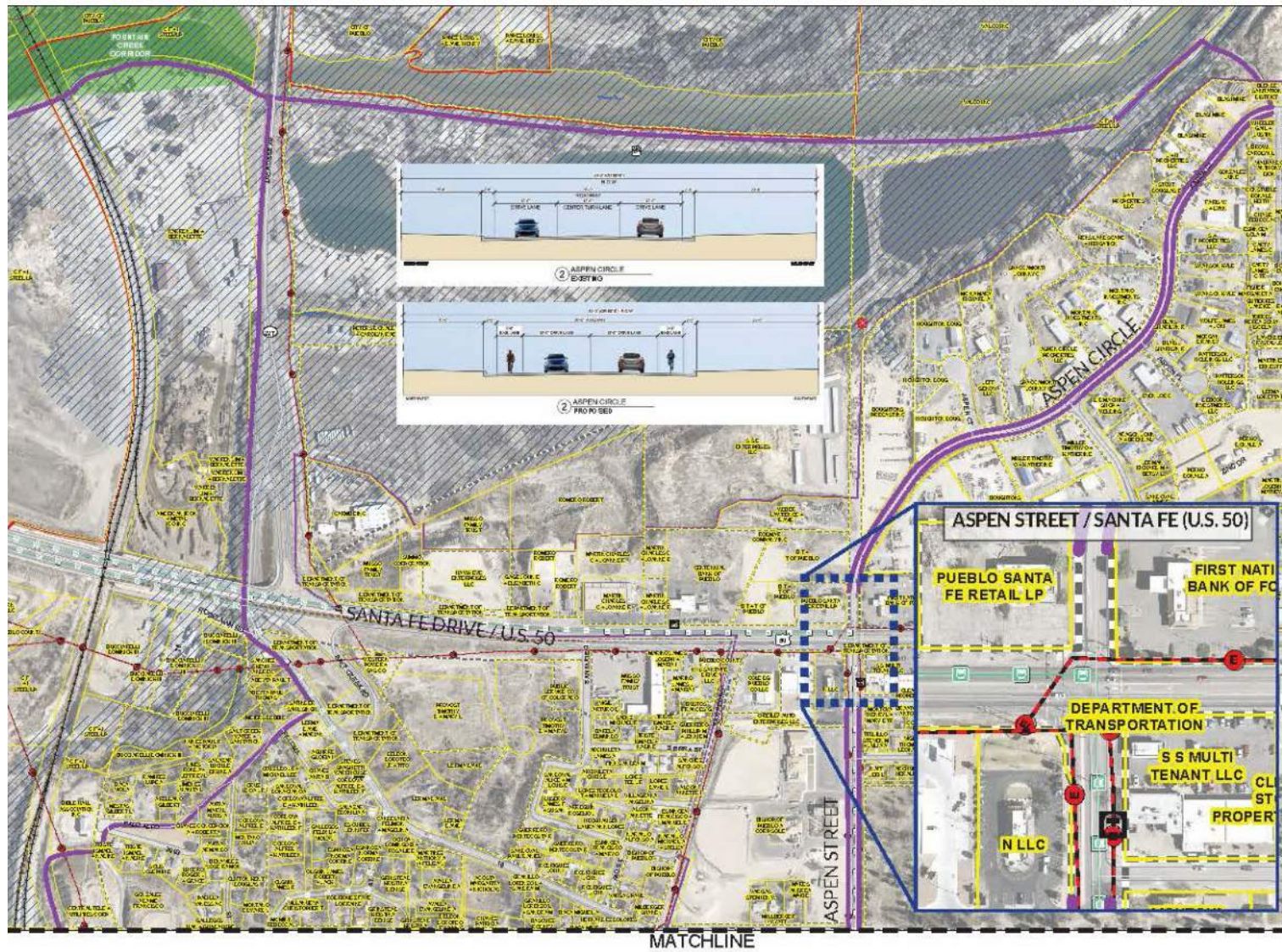
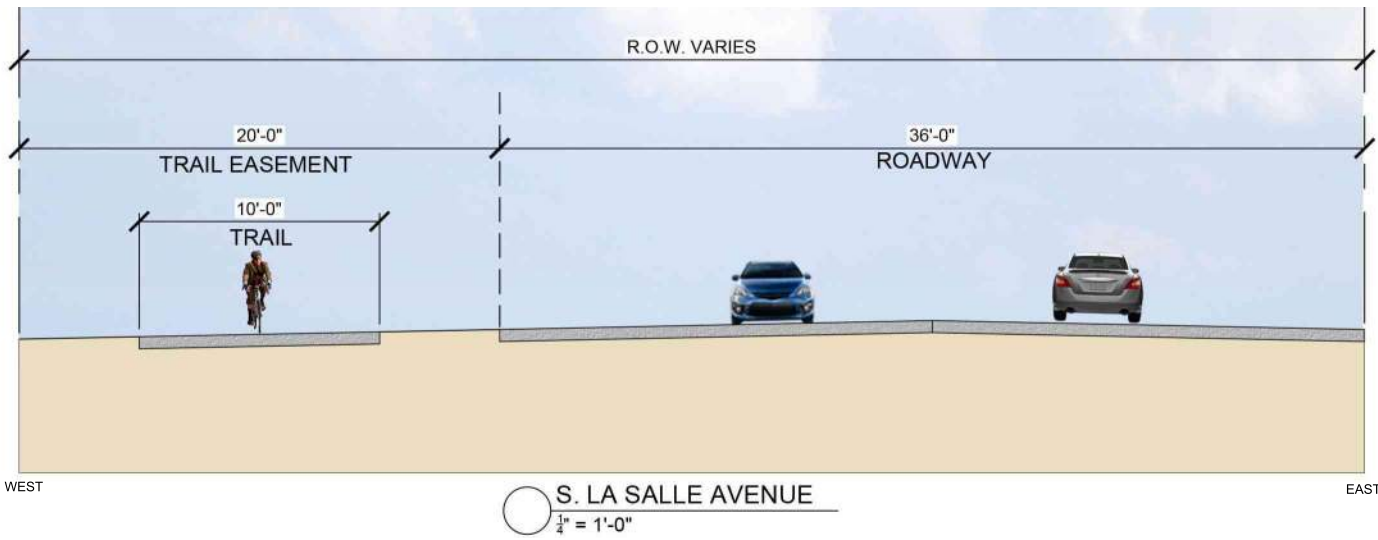
Central Telephone & Utilities:	203 LF = 2%
Gonzalez, Adame Francisco:	103 LF = 1%
EVRAZ Steel:	3,925 LF = 45%
Colo + Southern RR Co:	60 LF = 1%
Medina, George:	120 LF = 1%

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
SALT CREEK TRAIL	
REACH No. 1 (Arkansas River to LaCrosse Ave.)	\$49,835
REACH No. 2 (LaCrosse Ave.)	\$6,510
REACH No. 3 (LaCrosse Ave. to Palo Alto St.)	\$1,762
REACH No. 4 (Palo Alto St. to Bessemer Ditch)	\$595,405
SUBTOTAL	\$653,512
Design Fee, Testing, Traffic & Erosion Control	\$196,054
Contingency - 20%	\$130,702
TOTAL	\$980,268

Primary Issues

- CDOT La Crosse Avenue (SH 227) SB Lane Adjustment/Restriping
- Private Property Public Access Easements



**Salt Creek Trail
Aspen Street / Circle On-Street Bike Lanes**

Pueblo River Trails Extension Master Plan

October 1, 2020

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Aspen Street / Circle On-Street Bike Lanes

Context

The Aspen Street On-Street Bike Lanes are located in the most dense transportation infrastructure location of the St. Charles Mesa. The Pueblo County Gym (located on the former County Farm property), the Fulton Heights Community Center, and Santa Fe (US 50C) commercial are all served by this On-Street route.

Role

These On-Street Bike Lanes provide a direct, commuter connection between the Arkansas River Trail and the south St. Charles Mesa with connections to the east on the Bessemer Ditch Trail and County Farm Road Bike Lanes. They are intended to serve experienced bike riders, who are comfortable riding in traffic, with a fast, direct north/south commuter connector. The Aspen Street On-Street Bike Lanes intersect with the Santa Fe (US 50C) east/west On-Street Bike Lanes further extending the access reach of bike access from that corridor.

Alignment

The Salt Creek Trail Segment starts at the The Aspen Street On-Street Bike Lanes start at the proposed Arkansas River Trail connection, located at the Blende Sanitation District property, at the north end of Aspen Circle, in the Aspen Industrial Park. Bike Lanes are proposed on each side of the existing Aspen Circle roadway (36' width) The relatively low traffic volume on this portion of the roadway is served by existing lane designations of two (2) travel lanes (12' width) and a center turn lane (12' width). The center turn lane is primarily to accommodate wide turning radii of large trucks entering/exiting industrial properties located in the industrial park. The proposed lane restriping to add designated On-Street Bike Lanes will not reduce the pavement width necessary for truck turning movements in the industrial park.

The intersection of Aspen Street/Circle with

Santa Fe (US 50C) however, offers a significant challenge for bicycle and pedestrian crossing. Double, left turn lanes from the south to accommodate NB to WB traffic volumes has caused the roadway width to be expanded within the existing ROW, leaving insufficient room for standard sidewalk widths on each side. A similar condition exists on the north side of the intersection, due to necessary alignment of through lanes, even though there is only one SB to EB left turn lane. At this signalized intersection, north/south crossing is limited to the east side of Aspen Street/Circle, requiring pedestrians on the west side of Aspen Street/Circle to cross to the east side, before crossing Santa Fe. Ultimately, the solution to improving pedestrian and bicycle safety at this intersection will require acquisition of additional street ROW. Until then cyclists will need to merge with vehicular traffic through the intersection, or dismount and use the narrow existing sidewalks and pedestrian crossing.

One block south of the intersection, beyond the left turn lanes, Aspen Street returns to a three (3) lane (36' width), three lane roadway consisting of two (2) travel lanes (12' width), and a center turn lane (12' width). Our understanding is that the 36' width and lane designation was a required improvement for construction vehicles during construction of the power plant to the south, that has now been completed. Street restriping is proposed to create two (2), travel lanes (13' width) with two (2) On-Street Bike Lanes (5' width), with one on each side.

Trail Components

This Trail Segment is approximately one and one half miles (1.5 mi.) in length and is composed exclusively of On-Street Bike Trails (5' width).

Property Ownership

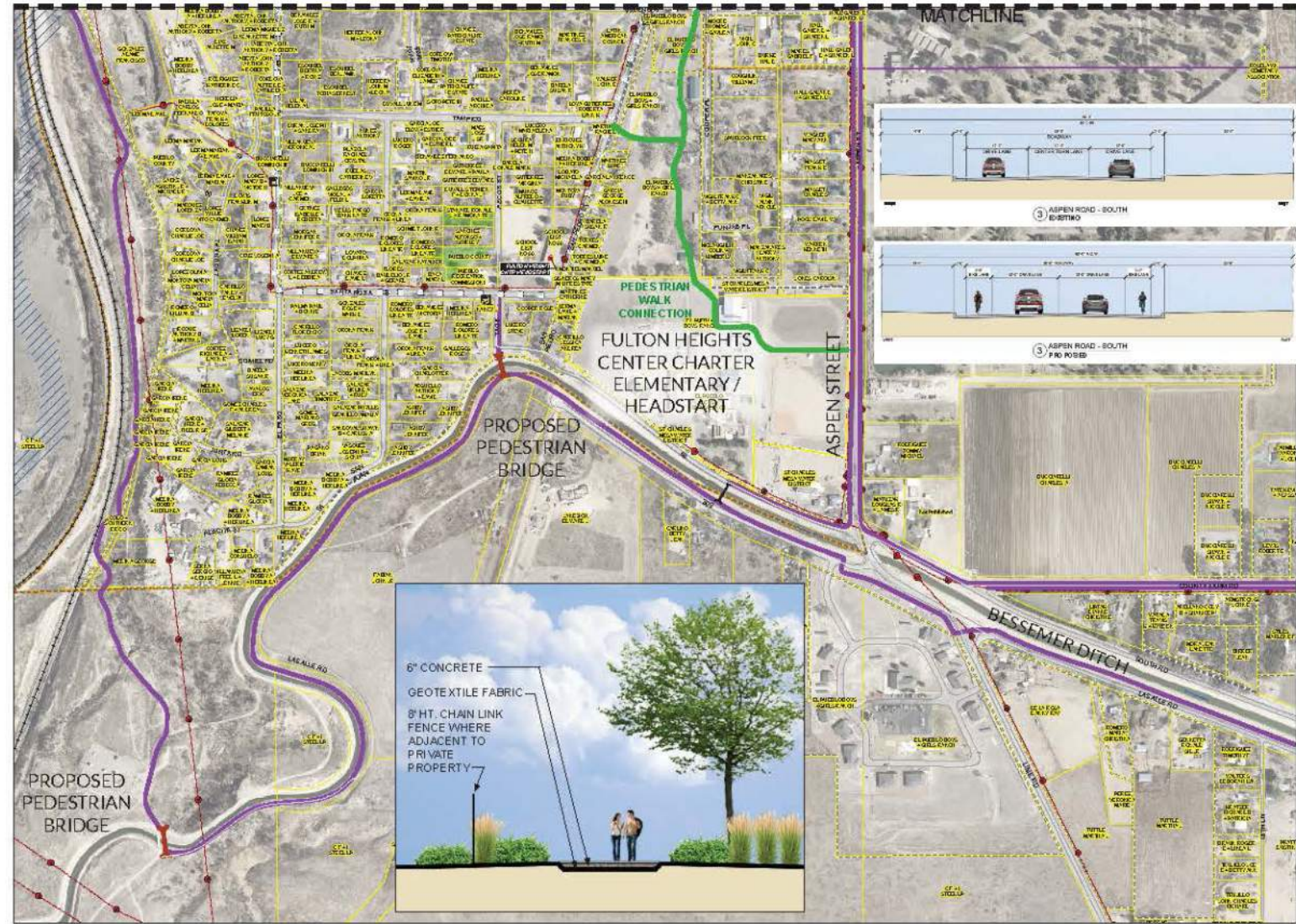
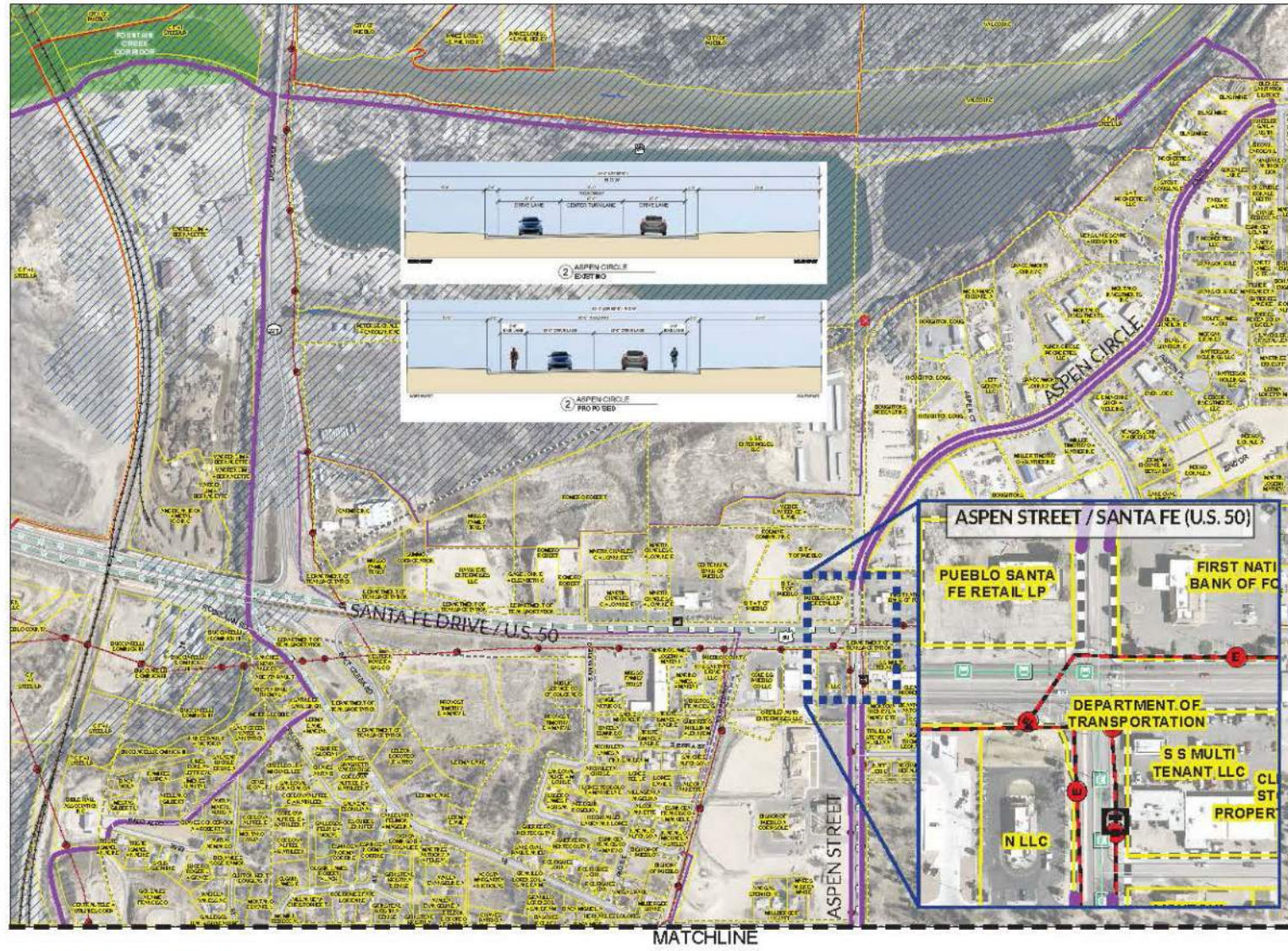
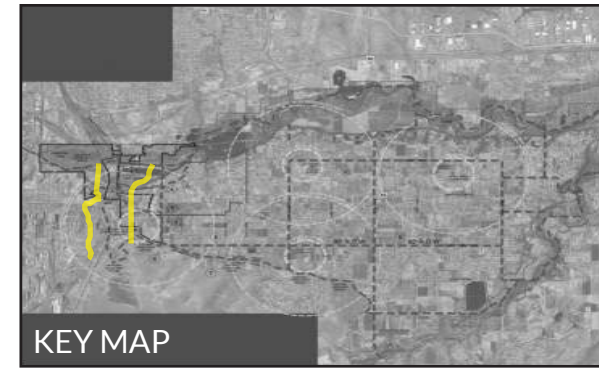
Public ROW: 7,668 LF = 100%

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
ASPEN CIRCLE / STREET	
Aspen Circle / Street	\$10,762
Design Fee, Testing, Traffic & Erosion Control	\$3,229
Contingency - 20%	\$2,152
TOTAL	\$16,143

Primary Issues

- CDOT La Crosse Avenue (SH 227) SB Lane Adjustment/Restriping
- Private Property Public Access Easements



**Salt Creek Trail
Aspen Street / Circle On-Street Bike Lanes**

Pueblo River Trails Extension Master Plan

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Bessemer Ditch Trail

Context

Constructed in the late 1800's, the Bessemer Irrigation Ditch pre-dates most residential development on the St. Charles Mesa and delivers most of the water that made the Mesa's agricultural beginnings possible. As such an important predecessor to current development, it represents an historic resource that should be honored and respected.

The Bessemer Ditch is generally perceived as the south edge of the St Charles Mesa residential neighborhoods, even though residential enclaves exist beyond it, to the south. Its prominence as a dividing line is largely due to the limited number of street crossings that have been allowed along its course. The absence of street crossings makes it a very attractive multi-use trail corridor.

Role

The role of the proposed Bessemer Ditch Trail in the regional trail network is to provide an east/west extension of the trail network that is safe for pedestrians and bicyclists of all levels of experience, that are best served by a trail route that has limited vehicular interruptions. Its historic role in development of the St. Charles Mesa should be celebrated and recognized as an important historic and cultural resource.

Alignment

The Bessemer Ditch Trail alignment is proposed to start at the proposed Salt Creek Trail connection, on the west, eastward to the point where the La Crosse Avenue ROW and the Ditch ROW separate. Future extension as far as Manning Road is envisioned in long range planning. A public access easement would be necessary for the trail to exist within the Bessemer Ditch ROW.

The alignment is proposed on the south side of the ditch because there is greater separation between the ditch channel and existing

roadway improvements, allowing for more room to accommodate Off-Street Trail (10' Concrete) improvements, while maintaining a thirty-foot (30') setback from the ditch for safety and maintenance operations.

While the south side of the ditch provides a safer alignment, the majority of trail users reside on the north side of the ditch; therefore, four new pedestrian/bicycle bridges are proposed to provide crossings of the channel at regular intervals and key connection points to the north.

Trail Components

This Trail Segment is approximately two and one half miles (2.5 mi.) in length and is proposed exclusively of 10' width concrete, Off-Street Trail. Almost the entire trail route is located in the Bessemer Irrigation Ditch Company ROW. Four (10'width) pedestrian bridges are proposed over the ditch channel. Limited grading, drainage and site preparation will be required.

Property Ownership

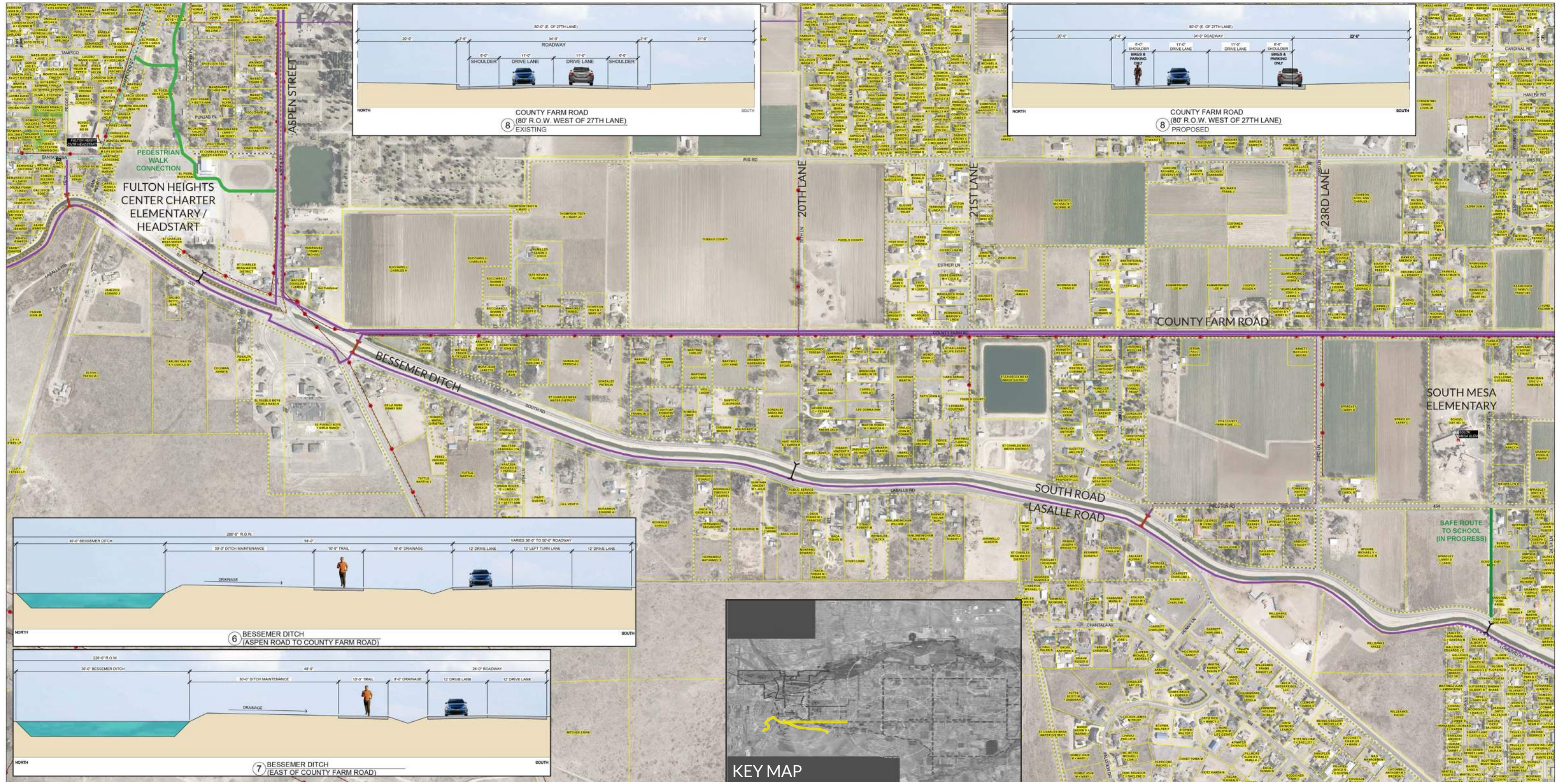
Private Ownership:
 Bessemer Ditch Company ROW:
 12,410 LF = 96%
 EVRAZ Steel: 531 LF = 4%

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
BESSEMER DITCH	
Bessemer Ditch	\$1,959,723
40' Pedestrian Bridge Cost	\$523,930
TOTAL	\$2,483,653
Design Fee, Testing, Traffic & Erosion Control	\$745,096
Contingency - 20%	\$496,731
TOTAL	\$3,725,479

Primary Issues

- Bessemer Ditch Company Public Access Easements



Bessemer Ditch Trail County Farm Road On-Street Bike Lanes

Pueblo River Trails Extension Master Plan

October 1, 2020

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County Farm Road On-Street Bike Lanes

Context

The County Farm Road On-Street Bike Lanes provide a continuous east/west corridor across the St. Charles Mesa, south of Santa Fe (US 50C), through relatively rural land uses that include larger lot residential development. Existing improvements include the most improved roadway section (34' width) across the south Mesa with curb and gutter drainage improvements, and improved roadway pavement. The larger lot homes often include long driveways, and areas for visitor parking. The relatively lower densities of the area result in lower traffic volumes.

Role

The County Farm Road On-Street Bike Lanes' role in the regional trail network is to create a multi-modal corridor that spans the entire south St. Charles Mesa from its eastern edge to the Aspen Street (north/south) corridor on the west.

Alignment

The proposed County Farm Road On-Street Bike Lanes start at the intersection of County Farm Road with Aspen Street, extending eastward to 30th Lane. The existing street ROW west of 27th Lane is 80' in width, and east of 27th Lane, it reduces to 60' in width. Roadway Pavement on both sides of 27th Lane is 34' in width comprised of two (2) travel lanes (11' width) and two (2) striped roadway shoulders/on-street parking lanes (6' width).

Proposed improvements include re-designation with surface/signage markings to identify the shoulder lanes as On-Street Bicycle Lanes with on-street parking permitted. Due to the relatively low on-street parking demand (due to larger lot vehicle accommodations), and relatively low traffic volume, this designation is intended to balance the needs between multi-modal and vehicular users.

Trail Components

This Trail Segment is approximately four miles (4 mi.) in length and is proposed exclusively of re-designation of existing non-specified shoulder striping (6' width) as On-Street Bike Lanes with on-street parking permitted.

Property Ownership

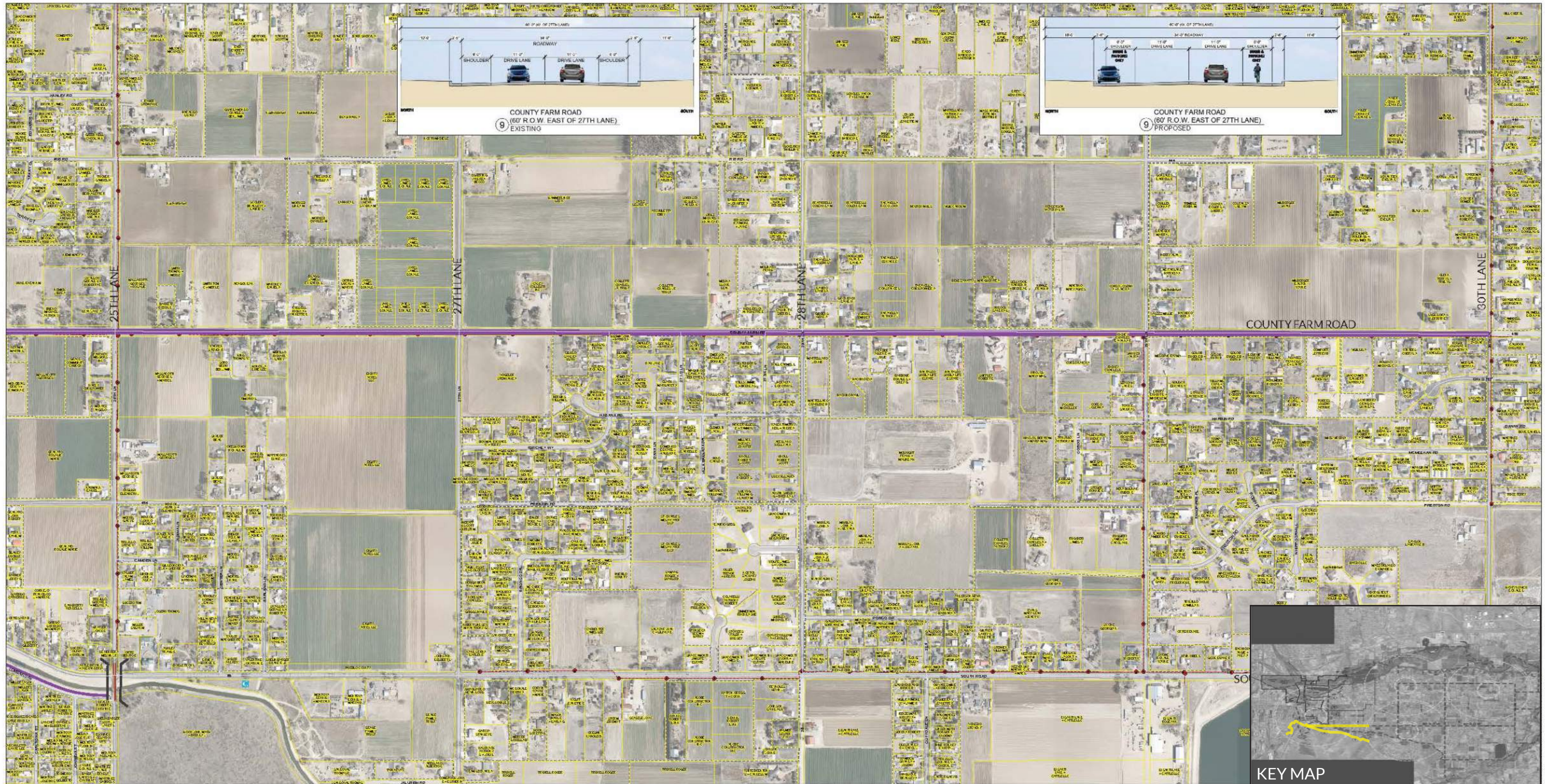
Public ROW: 21,173 LF = 100% Estimated Cost

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
COUNTY FARM ROAD	
County Farm Road	\$32,342
Design Fee, Testing, Traffic & Erosion Control	\$9,703
Contingency - 20%	\$6,468
TOTAL	\$48,513

Primary Issues

- none



County Farm Road On-Street Bike Lanes

Pueblo River Trails Extension Master Plan

Disclaimer: PRELIMINARY - For Draft Master Planning Study Purposes Only



October 1, 2020



Everett Road On-Street Bike Route (Sharrows)

Context

The Everett Road On-Street Bike Route (Sharrow) Segment extends east/west across the entire North Mesa neighborhood providing the only continuous, improved street across the St. Charles Mesa, north of Santa Fe (US 50C). As such, it serves the growing single family detached residential population in this area of new, infill development, and is located between the two school campuses in this neighborhood with very few existing, continuous sidewalks.

Role

The role of this On-Street Sharrow Segment in the regional trail network is to provide a continuous east/west bicycle connection across the northern St. Charles Mesa neighborhood, where most east/west streets are constrained by narrow pavement. It extends between Pleasant View Middle School and North Mesa Elementary School, offering an on-street connection to those destinations. As the east/west roadway with the most well-developed, continuous street cross section that spans the North Mesa Neighborhood, it provides an existing, improved route between future trail network connections to the South Mesa, on 23rd Avenue (west) and 30th Avenue (east). Future improvements to the south are identified in the long-range trails network, but require street improvements prior to accommodating safe, mixed vehicular/bicycle travel.

Alignment

The Everett Road Sharrow Segment starts at 23rd Avenue on the west edge of Pleasant View Middle School campus and extends approximately two miles (2 mi.) to 30th Lane, one half block north of the North Mesa Elementary School campus.

Trail Components

This Segment is approximately two miles (2 mi.) in length and is proposed exclusively of signage and pavement markings to alert

vehicles of the presence of bicyclists in this Share-the-Road designation. No grading, drainage, site preparation or pavement improvements will be required.

Property Ownership

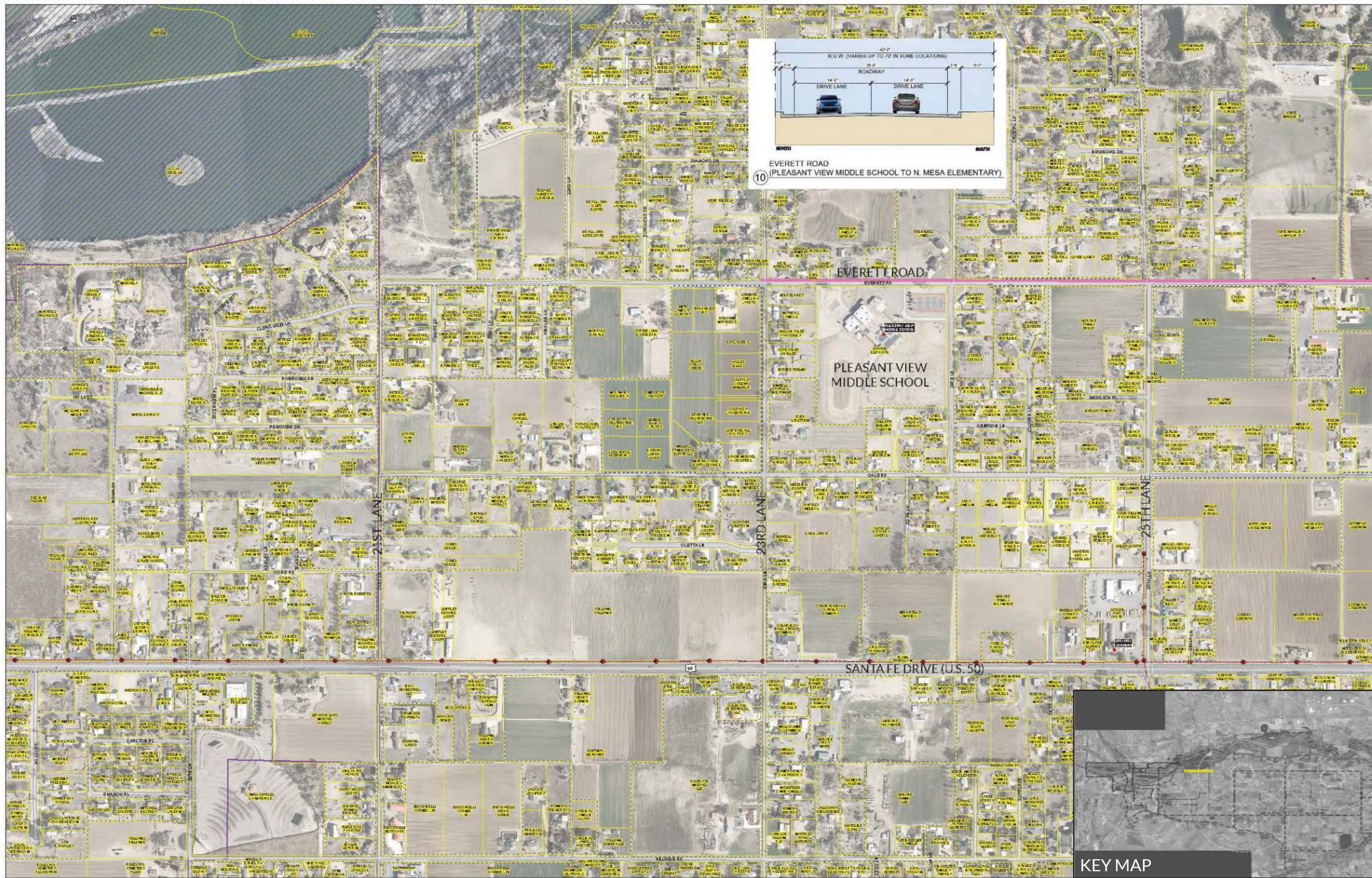
Public ROW: 10,571 LF = 100%

Estimated Cost

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN	
EVERETT ROAD	
Everett Road	\$16,600
Design Fee, Testing, Traffic & Erosion Control	\$4,980
Contingency - 20%	\$3,320
TOTAL	\$24,900

Primary Issues

- None



Pleasant View Middle School / Everett Road

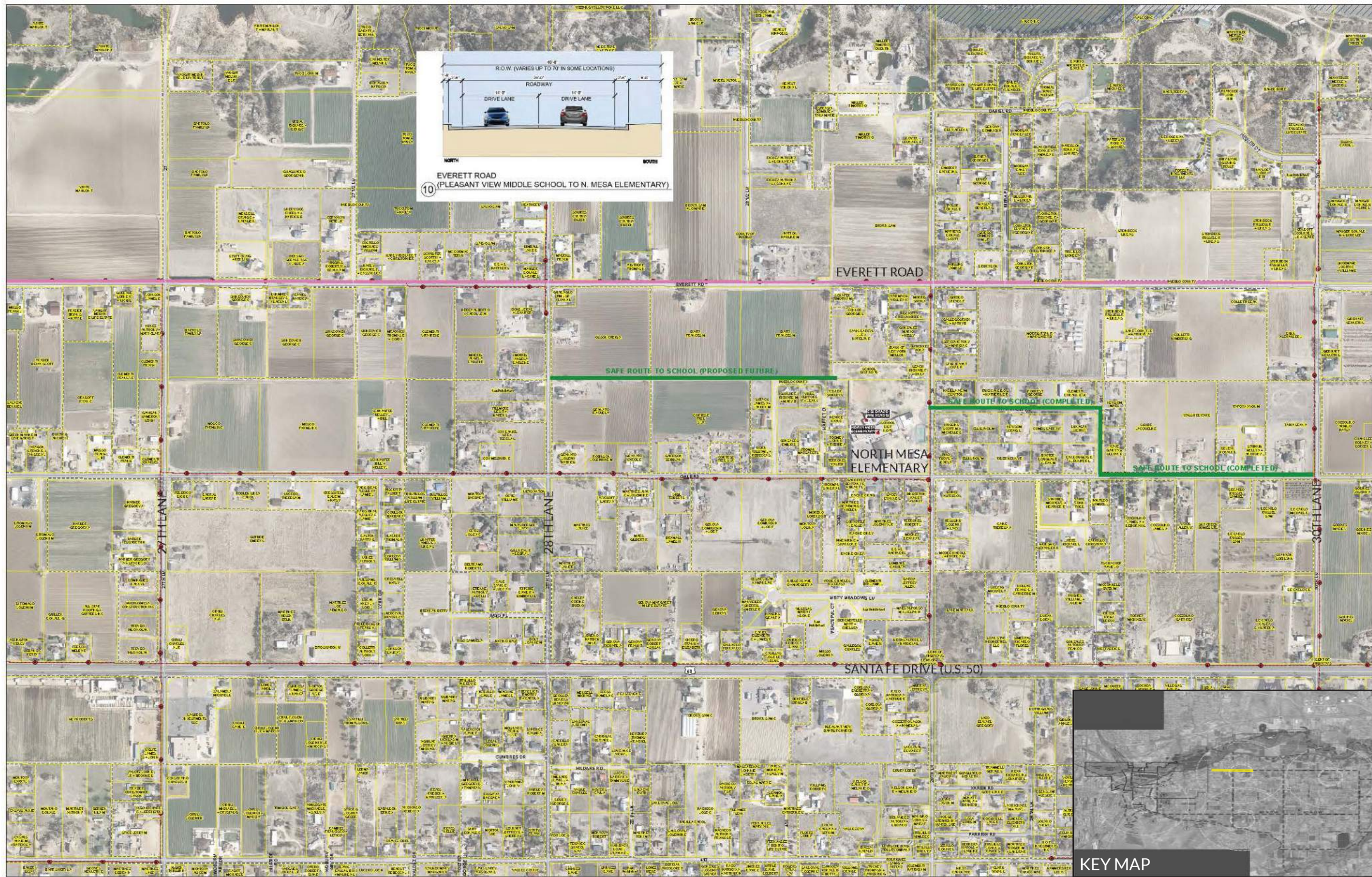
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October 1, 2020





North Mesa Elementary School / Everett Road

Pueblo River Trails Extension Master Plan

October 1, 2020



Public Engagement Program

Public Engagement Overview

The Public Engagement Program for the Pueblo River Trails Extension Master Plan Update was structured to introduce the 1998 Master Plan, seek information about site conditions that had changed since that study had been completed, learn about citizen needs wants and desires, and provide an opportunity for public review and input as the master plan update process progressed. The program was organized to include three (3) approaches to report progress and garner input:

1. Public Open House Meetings
2. Stakeholder Meetings and Interview
3. Pueblo Board of County Commissioners Public Work Session

Public Open House Meetings

A series of four (4) Public Open House Meetings were hosted at North Mesa Elementary School in the Library/ Media Center.

Open House 1 – November 12, 2019

Project Introduction, Master Plan Process, Vision, Goals and Objectives, Opportunities and Constraints

Open House 2 – December 10, 2019

Alternative Trail Routes and Proposed Trail Priorities

Open House 3 – January 14, 2020

Preferred Trail Priorities and Cost Estimates

Open House 4 – January 21, 2020

Final Master Plan Trails

Public Outreach

The Public Open House Meeting schedule was announced and advertised through local on-line and telecommunications including schedule posting on the Pueblo County Website, the PuebloPACE Face Book Page, and automated telephone announcements on the Pueblo School District 60, North Mesa Elementary School weekly School Messenger system.

Stakeholder Meetings

Project Stakeholders were identified as trail system enthusiasts and interest groups, school principals and key, large property owners in the project area, whose property was being crossed by proposed future trail alignments identified in the 1998 Trails Study. Stakeholder representatives were invited to attend the Project Kick-off meeting on April 25, 2019, to introduce / reintroduce them to the 1998 Study and the Master Plan Update project.

Stakeholders were invited to subsequent project team meetings. Participating stakeholder representatives included:

- Evraz North America (formerly CF&I Steel) – Leaha Zinanti, Senior Administrative Assistant
- Valco, Inc. - Tom Brubaker
- Donald Scanga, CDOT Region 2
- City of Pueblo Transportation Planner and former PACOG Planner -Bart Mikitowicz
- City of Pueblo Acting Planning Director and 1998 Study Participant, Scott Hobson
- Steve Smith, PuebloPACE
- Mark Perry, St. Charles Mesa Resident and Parent
- Jeff Howes, Principal, North Mesa Elementary School

Subsequent one-on-one interviews or meetings were held with stakeholders that were not able to attend regularly scheduled meetings, including:

- Evraz North America (formerly CF&I Steel) – Leaha Zinanti, Senior Administrative Assistant
- Bessemer Ditch Company – Mike Hill, Superintendent
- PuebloPACE – Kim Arline, Pedestrian and Bicycle Advocate
- Blende Sanitation District - Johnny Spaccamonti
- American Iron and Metal - Jim and Bernadette Warren, Owners

Board of County Commissioners Public Work Session

The Final Master Plan document was submitted to the Pueblo Board of County Commissioners (BOCC) for review and comment on October 1, 2020. A Public BOCC Work Session was held on Tuesday, October 13, 2020, for the consultant team to present the Master Plan, respond to Commissioner questions and comments, and receive input on additional information or edits. A second BOCC Work Session presentation was made Tuesday, November 17, 2020 to respond to Commissioner questions and make clarifications. Due to COVID restrictions, the Work session was conducted as an on-line ZOOM meeting.



Trail Management

PUEBLO COUNTY RIVER TRAIL EXTENSION PRELIMINARY DESIGN AND ENGINEERING STUDY MATRIX PROJECT NO. 18.710.002

TRAIL MANAGEMENT CONSIDERATIONS

Management Approach and Philosophy

Once completed, the Pueblo County River Trail Extension will represent a substantial investment as an amenity in the community. Hopefully, it will endure for generations to come. Well-managed and maintained, the trail can be a point of pride for the community. More importantly, it can be a safe and pleasant place. Providing effective operations and maintenance, however, requires a commitment of time and resources. There should be a comprehensive management plan to carry out operations and maintenance activities and a long-term commitment of resources by the community. The following text offers a framework for an effective operations and maintenance program.

Operational Considerations

An effective management plan should consider at least the following areas:

- Maintenance
- User Safety and Risk Management
- Programming and Events
- Stewardship and Enhancement
- Protection of Adjacent Private Property Interests

Maintenance

Good maintenance begins with good planning and design, followed by quality construction of the built components (the Maintenance Checklist at the end of this section outlines common maintenance activities). Initial investment in quality design, construction materials, and installation will pay off many-fold over the long term. An effective, on-going maintenance program is also essential. Overall, there are two kinds of maintenance to consider—routine and remedial.

ROUTINE MAINTENANCE refers to correcting significant defects as well as repairing, replacing or restoring major components that have been destroyed, damaged, or significantly deteriorated during the life of the project. Examples of remedial maintenance include stabilization of a severely eroded hillside or replacing a bridge lost in a flood. Ideally, remedial maintenance will be part of a long-term capital improvements plan.

Several areas of maintenance should be considered:

1. Trail Maintenance

The trail system must be kept in good repair. First, the main multi-use trail will require sweeping to remove accumulated loose gravel, any broken glass or thorns which can cause flat tires. Unpaved and primitive trails will need regular repair of minor washouts and other damage. Underpasses must be kept free of silt and debris. Any granular stone sections will need periodic patching, vegetation control, and edge repair.

Maintenance crews will need to address snow and ice build-up. Routes ultimately designated for year-round commuting and wheelchair access should be plowed. Surface drainage components on all trails including culverts, ditches and swales should be kept in good operating condition including removal of debris from inlets and culverts. Maintenance crews should also address any problems caused by unchecked runoff such as puddles and washouts. Safety railings, pedestrian bridges, signage and other components must also be kept in good repair, including periodic structural inspections by a structural engineer to assess any deterioration or damage.

It is important to trim overhead vegetation for adequate head room. Multi-use pedestrian/bike paths should have a

minimum of 8'6" of head clearance. For equestrian use, provide a minimum of 10' of clearance. Large, dead overhanging limbs or nearby dead trees that might fall on the trail, should be routinely inspected and, when necessary, trimmed or cleared away. Ideally, a two to five-foot wide swath should be mowed along both sides of the multi-use trails to create a shoulder for joggers and horses. The mowed shoulder also creates a groomed look making the trail more inviting. This may not be appropriate on the primitive trail segments or in areas where preservice riparian vegetation is critical.

2. Disruptions and Detours

It is important that all public and private projects that disrupt trails, sidewalks, bike lanes and other routes of non-motorized travel provide a clearly marked, safe detour which is suitable for bicycles, pedestrians and people in wheelchairs. This consideration is all-too-often overlooked, treating non-motorized users as "second-class citizens" who must fend for themselves when a street, sidewalk or when the trail is closed. Success of a detour system calls for both a reporting procedure that assures that responsible agencies are aware of the closings and having appropriate detour procedures in place.

3. Graffiti and Vandalism Control

Project components such as benches, signs, railings, lamps, and picnic tables should be both durable and easy to repair or replace. Maintenance personnel should have a policy of quick repair or replacement when items are damaged. If possible, each responsible maintenance agency should store spare parts and replacement components. Again, there should be regular monitoring for damage so that action can be taken quickly.

Graffiti treatments include removal by chemical or sandblasting; special graffiti-

proof surfacing; and painting over. Painting over generally works best and this suggests using painted surfaces to begin with. For example, a concrete wall can be given a coating of a product such as "Thoroseal" (a special durable concrete paint) or epoxy sealant, rather than an exposed aggregate surface. Maintenance crews simply paint over the graffiti or easily remove it from coated surfaces.

4. Trail Corridor Vegetation Management

In general, the trail should feature low maintenance, natural landscapes that are inviting to the user. This might be achieved over time using native grasses, wildflowers, selective weed removal, and in parks and more formal areas, articulated mowing. Articulated mowing means shaping a natural landscape by grooming the trail shoulders and selectively creating mowed meadows and sweeps along the corridor. Under such a regime, noxious and undesirable weeds should be identified and removed—ideally by cutting or mechanical removal rather than chemical application.

Finally, it is important to manage vegetation for user security. Maintain good lines of sight, user surveillance and escape routes. Avoid blind thickets close to the trail where a person could hide. These areas may pose or appear to pose a threat to users.

5. Trail Mile Markers and Street Location Signs

The installation of mile markers is suggested along trails. In addition to telling users where they are and how far they've gone, they can help maintenance people locate problems, help police and rescue personnel determine where an accident or incident has occurred, and index problem spots or needed remedial maintenance. Placing major street and road crossings should be clearly signed. Again, this helps both users and rescue personnel identify a location.

Trail Management (continued)

6. Connecting On-Street Routes

On-street bicycle and pedestrian access routes connecting to the trail must be kept in good operating condition including pavement repair, good drainage, properly functioning signs and signals, clear pavement markings and other elements. In addition, there are special bicyclist needs. These include: attention during street sweeping to remove debris and grit which tend to accumulate in the gutter area (where bicyclists usually ride), regular street sweeping of designated Bike Lanes and, conscientious repair of chuck holes and cracks to assure a smooth surface suitable for bicycling.

7. Stream Channel Maintenance

Portions of the Pueblo County River Trail Extension cross several tributaries/ditches. Stewardship of these resources is important. This includes care of the channel bed, stream banks and related structures. Activities include: removal of trash and debris (note: “debris” means material that can pose a flood hazard by clogging bridges and other constricted areas. Most natural debris such as fallen trees and detritus is beneficial to wildlife and should be left if at all possible); litter collection; erosion control and repair; and maintenance of any trail-associated in-stream improvements. Note that the bike paths can also serve as “access roads” for channel maintenance. Be sure, however, that such access by maintenance vehicles is kept to a minimum and that speeds are controlled to assure user enjoyment and safety.

Special Attention should be given to gates, fences, and cattle guards. Regular maintenance will assure that gates, locks and guards function correctly and fences are mended, protecting private property, trail users and controlling cattle grazing

and unauthorized private property access.

8. Maintenance Inventory and Schedule

The maintenance agency should keep a checklist and schedule which addresses both routine and remedial maintenance functions. The checklist should itemize all maintenance functions and provide a way to report problems and their location. There should also be a way to log the outcome of each reported maintenance problem. These reporting sheets should be kept on file and periodically reviewed by maintenance supervisors to determine if the problems were solved and to identify any trends or problem areas.

The maintenance program should include a public monitoring program that offers a way for citizens to report and receive feedback on maintenance problems, a procedures manual for performing maintenance functions, and a projected total annual cost of the program.

9. Patrol and Emergency Procedures

Patrols are an essential element of trail operations and maintenance. Along the trail, patrolling personnel can provide a number of services including: helping to prevent vandalism and other crimes against persons or property; handling injuries and medical emergencies; arranging for rescue; directing evacuation in the event of flooding or other problems; and disseminating information to trail users.

Trail Patrol can be handled in several ways. Patrols could be by law enforcement agencies including regular visits by police officers (bicycle or horse-mounted police), or by a special trail ranger group. The trail rangers might be trained by law enforcement personnel although seasonal

employees who merely advise and assist may suffice. Trail patrol personnel should travel on foot, horseback or bicycle and ideally carry a two-way radio or cellular phone. If trail rangers are used, they should have back-up by police including who routinely patrols the corridor.

There should also be provision for medical treatment of injured or incapacitated trail users. This includes emergency treatment as well as non-emergency first aid. Treatment could be provided by either full or part-time personnel. For this reason, all patrol personnel (even seasonal employees) should be certified in first aid and cardio-pulmonary resuscitation (CPR). There should be an emergency evacuation plan prepared in conjunction with law enforcement, fire and medical center personnel. The plan should define points of access, safe and effective routes for emergency vehicle operations, load limits for bridges and boardwalks along the trail (ambulances and rescue vehicles may weigh as much as 12,000 pounds), distance in minutes and miles from trail access points to local hospitals and other medical facilities, and jurisdictional responsibility of police, fire and medical agencies. The emergency plan should be graphically illustrated and posted at the entrance of the trail or trail system so that users will see it and have immediate access to essential telephone numbers and other critical information. Ideally, there should be emergency phones along the trail. (The plan and amenities should be developed with the aid of local law enforcement.)

User Safety and Risk Management

It is important to assure a safe and enjoyable experience for users and, at the same time, minimize liability exposure. Total safety and protection from possible lawsuits can never be guaranteed. The responsible jurisdictions should strive to minimize problems by

adhering to a basic standard of care to assure that avoidable hazards are avoided and that the unavoidable ones are clearly marked and known to users.

User safety should be considered at the beginning of the design process. Off-street bike corridors, trails and other components should meet state-of-the-art engineering standards as described in the American Association of State Highway and Traffic Officials (AASHTO) and Colorado Department of Transportation (CDOT) guidelines. All plans, designs and maintenance procedures should be reviewed by a range of experts including maintenance personnel, police and fire/rescue personnel, user groups and the public. Anticipate problems and try to solve them ahead of time.

A risk-management program should be put in place that includes:

- routine inspection of facilities;
- a reporting and response program that allows both citizens and maintenance staff to identify problems and which promotes a quick response to remedying those problems;
- a cooperative record-keeping system with local law enforcement and rescue departments so that recurrent problems can be identified and addressed;
- a program of regular internal staff review of operations and maintenance procedures to assure hazards are being properly addressed.

Stewardship and Enhancement

There should be attention given to on-going protection, renewal, refinement, and embellishment of the Pueblo County River Trail Extension environment. Some issues to consider include protection of historic structures, expanding and protecting buffer zones along the stream and trail edges, addressing corridor flora and fauna, protecting views, reviewing the design of

new street crossings and utility structures and a myriad of other concerns.

There must be a permanent entity or group of agencies to watch over the trail corridor. Ideally, this entity includes an effective combination of agency staff, concerned citizens, and responsible elected officials. This group should continually review and respond to proposed actions by other agencies and property owners evaluating projects such as highway widenings, roadway crossings, encroachments, commercial development, utility lines and subdivisions which might compromise trail use or the resource protection objectives of the system.

Protection of Adjacent Private Property Interests

There should be attention given to on-The Pueblo River Trail Extension will pass adjacent to and through private property (where easements are granted). Residents, landowners, and businesses along the proposed trail corridors are understandably concerned about the impact of the trails on privacy, livestock, crops, security and wildlife on their lands. The management program must assure that these concerns are addressed and that negative impacts on adjacent property owners are avoided.

This can be accomplished in several ways including: a well-run maintenance program; privacy/security fences where required; and education of the public through pamphlets and informational signage highlighting the natural resources and the user's responsibilities. It is recommended that close contact be maintained with property owners through regular communication. It is also recommended that one or more property owners be represented on the entity formed to watch over the development, maintenance, and future of the trail.

Administration - Who is Responsible for What?

Because of the scale of the project, it is essential that Pueblo County Parks, Public Works and other interests establish a formal mechanism for

coordination. As mentioned above, a committee should be established consisting of key agency personnel, landowner representatives and citizen group representatives. There should be a mutually adopted management plan and the members should meet regularly to assure continued coordination and quality.

Feedback and Troubleshooting

Perhaps the most important component of a quality trail system is a way to get feedback from the public. There must be an effective monitoring system that includes a way for trail advocates, bicyclists, pedestrians and wheelchair users to report problems. This might be best accomplished by posting a "hotline" number for people to call when problems are noted. The hotline number could be posted in prominent locations such as on signs along the trail and in places like fast food restaurants, supermarkets and bike shops. It is essential that a person be designated to respond to and follow up on all legitimate complains. Pre-addressed post cards to a designated "Trail Coordinator" could also allow people to identify trouble spots. The expected maintenance costs for the Pueblo County Trail Extension, as shown in the Preferred Trail Segments, is \$31,000.00 per year, for off-street trails only. It is presumed that on-street trail maintenance will be covered in existing street maintenance budgets. This estimate does not reflect the above implementation plan or major long-term reconstruction needs. The 1-year and following 5 and 20-year costs are general planning estimates and do not account for final trail design and maintenance decisions or potential trail volunteer programs.

- Anticipated 5-year maintenance costs for off-street trails \$36,000.00
- Anticipated 20-year maintenance costs for off-street trails \$56,000.00

For the general purpose of forecasting long-term maintenance costs, a 3% per year inflation rate was used.

MAINTENANCE CHECKLIST

Maintenance Item	Times/Year	Comments
Inspection	12-18	This can be done by volunteers with a checklist (min. management).
Sweeping	12-18	Applies to paved trails. Use a vacuum sweeper.
Concrete Trail Upkeep	As Required	Patch, fix heaved sections.
Asphalt Trail Upkeep	As Required	Patch, seal, overlay, and rolling need depends on thickness and level of use.
Crusher Fine/Non-Paved Upkeep	As Required	Fill holes, fix wash-outs, smooth out.
Litter Pick-up	12-18	Can be supplemented by volunteers.
Erosion Control	As Required	Address silt-producing areas.
Weed Control	3	Remove noxious species, preferably by mechanical means rather than chemical.
Clear Silt From Low Underpasses	As Required	VERY IMPORTANT ITEM!
Snow/Ice Removal	5-10	Especially from shaded spots, underpasses, and cattle guards. Warning, do not use salt on concrete.
Mow Trail Shoulders	3-4/year	3' to 5' shoulder except in sensitive areas.
Graffiti Removal and Minor Repairs	As Required	Can be done by rangers or volunteers. Paint walls and vertical surfaces for color so graffiti can easily be painted over.
Toilets/Drinking Fountains	Weekly	Clean and service. Consider composting or portable chemical type toilets.
Police/Ranger Patrol	Daily	Also install emergency #'s on signs and phones. Have crime/accident reporting system, legal address system. Place mile markers every 1/2 mile and identify cross streets for emergency locating.
Detours/Disruptions	As Required	Provide alternative routes. Should have inter-agency coordination mechanism.

PARK AND FEATURES

Trailheads	As Required	Pick up trash, sweep parking areas, repaint. Suggest pack-in/pack-out trash policy.
Natural Areas	As Requested	Pick up litter, trash. Address any nuisance problems. Have specially trained managers.

Trail Management (continued)

DEPRECIATION

Major Repair/Replacement	50-year life	Covers the ultimate replacement of large components over very long term.
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Source; South Suburban Park and Recreation Dist.; City of Boulder Parks Dept.; Mile High Drainage Dist.; Denver Parks Dept.; Jefferson County Open Space.

TYPICAL RANGE OF OPERATIONAL COSTS (Further Confirmation Needed)

Note of Caution: These estimates are very general and may vary significantly depending on local conditions, level of maintenance and accounting techniques. They are intended as a rough benchmark and not for specific budgeting purposes.

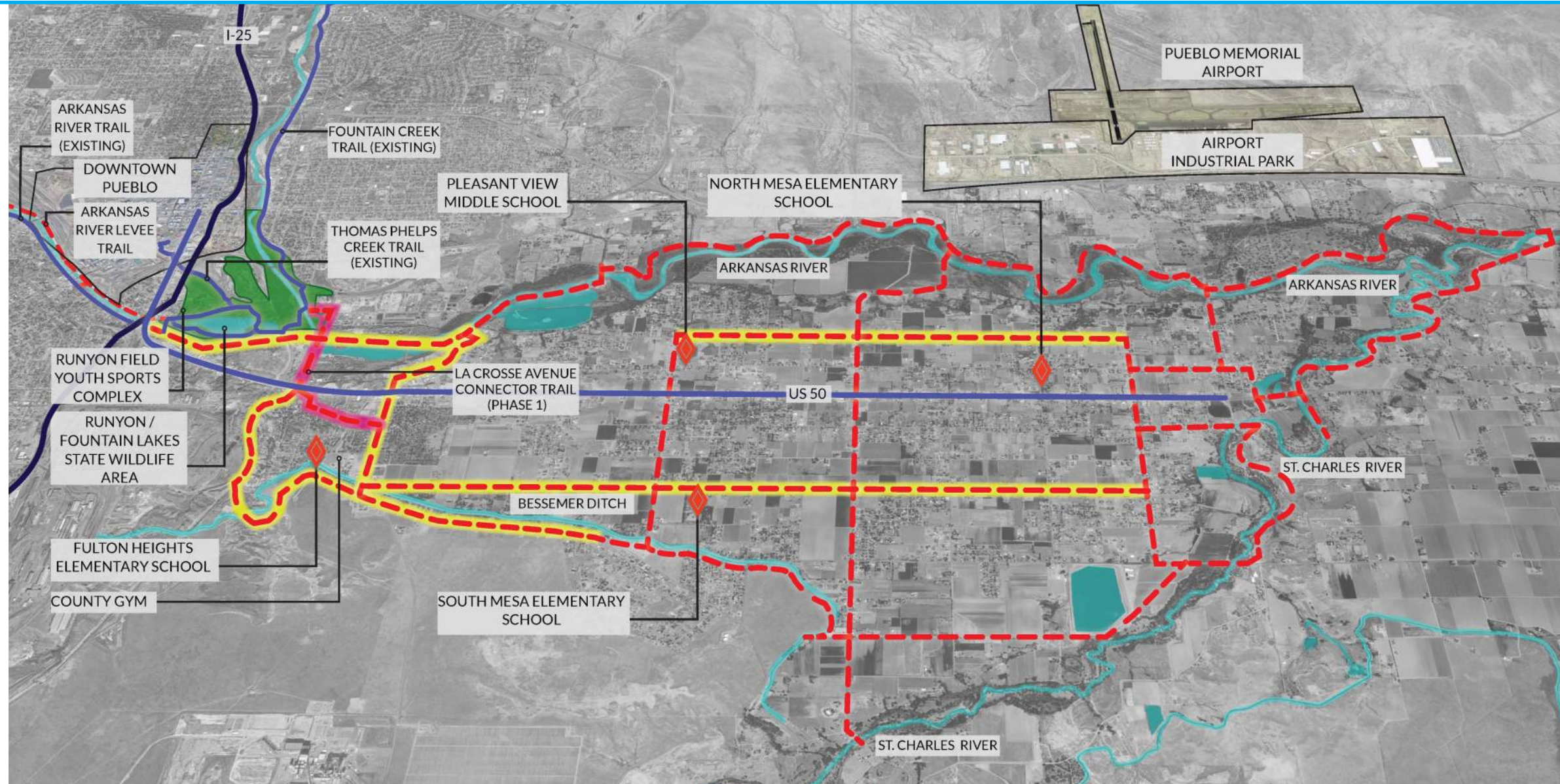
Maintenance Item	Unit Cost Per Year	Comments
Multi-Use Trail	\$2,450/mile	Sweep, mow edges, minor fix work. Crusher fines at low end; asphalt and concrete at high end.
Primitive Trail	\$(nominal) 100 to \$3,800/mile	Patrol, patch, fix water bars, vegetation management, etc. Some work can be done by volunteers.
Trailheads and Overlooks	\$4,400 to \$6,700/acre	Low end is for basic turf grass; high end for ballfields, shelters, and other upgrades included.
River/Stream Channel	\$800 to \$1600/mile/year	Debris removal, weeds, vegetation management, etc. Source: Mile High Drainage Dist.

Source: South Suburban Park and Recreation Dist.; Mile High Drainage Dist.; Denver Parks Dept.; Jefferson County Open Space;

It is presumed that On-Street Trail maintenance will be provided by the respective jurisdictional maintenance entities that are responsible for maintenance of the adjacent roadway. Additional Maintenance for on-street trails should be budgeted at \$2,640 / mile annually to accomplish pavement marking restriping every ten years.

For the general purpose of forecasting long-term maintenance costs, a 3% per year inflation rate can be used.

Pueblo River Trails Extension Master Plan 2020 Update



Appendix A - Technical Documentation

Cost Estimates

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 1 (TRAILHEAD PARKING to BRIDGE TRANSITION)

10 FOOT ON-STREET TRAIL BIKE LANE

ALTERNATE 1 - 10 FOOT WIDE MILL & PAVE TRAIL SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00240	2" MILL DEPTH	SY	547	\$3.22	\$1,761
411-03345	2" OVERLAY	TON	61.58	\$102.00	\$6,281
627-00005	RESTRIPE	LF	830	\$5.00	\$4,150
411-03342	6" SHOULDER PAVEMENT	TON	212.55	\$102.00	\$21,680
304-06007	8" SHOULDER CLASS 6 ABC	CY	138.55	\$57.13	\$7,915
608-00028	6" CONCRETE PAVEMENT	SY	73.88	\$99.32	\$7,338
TOTAL PER 1,000 L.F.					\$59,546
TOTAL PER L.F.					\$59.55

REACH No. 1 ALTERNATE 1 TOTAL

REACH No. 1 ALTERNATE 1 TOTAL	LF	825	\$59.55	\$49,126
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ALTERNATE 2 - 42 +/- FOOT WIDE MILL & PAVE SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00240	2" MILL DEPTH	SY	3752.11	\$3.22	\$12,082
411-03345	2" OVERLAY	TON	422.2	\$102.00	\$43,064
627-00005	RESTRIPE	LF	830	\$5.00	\$4,150
411-03342	6" SHOULDER PAVEMENT	TON	212.55	\$102.00	\$21,680
304-06007	8" SHOULDER CLASS 6 ABC	CY	138.55	\$57.13	\$7,915
608-00028	6" CONCRETE PAVEMENT	SY	73.88	\$99.32	\$7,338
TOTAL PER 1,000 L.F.					\$116,642
TOTAL PER L.F.					\$116.64

REACH No. 1 ALTERNATE 2 TOTAL

REACH No. 1 ALTERNATE 2 TOTAL	LF	825	\$116.64	\$96,229
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PUEBLO RIVER TRAILS EXTENSION MASTER PLAN LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 1 (TRAILHEAD PARKING to BRIDGE TRANSITION)

10 FOOT ON-STREET TRAIL BIKE LANE

ALTERNATE 3 - 10 FOOT WIDE REMOVE & CONCRETE PAVE SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00220	*REMOVE ASPHALT	SY	547	\$8.87	\$4,852
203-00000	*EXCAVATION	CY	60.2	\$32.13	\$1,934
304-06007	2" CLASS 6 ABC	CY	35.62	\$57.13	\$2,035
608-00028	6" CONCRETE PAVEMENT	SY	641.11	\$99.32	\$63,675
627-00005	RESTRIPE	LF	830	\$2.50	\$2,075
411-03342	6" SHOULDER PAVEMENT	TON	212.55	\$102.00	\$21,680
304-06007	8" SHOULDER CLASS 6 ABC	CY	138.55	\$57.13	\$7,915
TOTAL PER 1,000 L.F.					\$126,263
TOTAL PER L.F.					\$126.26

REACH No. 1 ALTERNATE 3 TOTAL

REACH No. 1 ALTERNATE 3 TOTAL	LF	825	\$126.26	\$104,167
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* Asphalt assumed depth 4" / Excavation 4" depth

LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 2 (BRIDGE SECTION & TRANSITIONS)

ON-STREET SHARROW W/ TRANSITIONS

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
411-03342	2" MILL DEPTH	SY	14.63	\$102.00	\$1,492
304-06007	2" OVERLAY	TON	9.53	\$57.13	\$544
627-00005	RESTRIPE	LF	830	\$10.00	\$8,300
TOTAL PER 1,000 L.F.					\$12,454
TOTAL PER L.F.					\$12.45

REACH No 2 BRIDGE SECTION & TRANSITIONS TOTAL

REACH No. 2 TOTAL	LF	830	\$12.45	\$10,337
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**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 3
(BRIDGE TRANSITION to GUARDRAIL SECTION)**

10' ON-STREET TRAIL

ALTERNATE 1 - 10 FOOT WIDE MILL & PAVE TRAIL SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
411-03342	2" MILL DEPTH	SY	973.33	\$3.22	\$3,134
304-06007	2" OVERLAY	TON	109.52	\$102.00	\$11,171
627-00005	RESTRIPE	LF	876	\$2.50	\$2,190
TOTAL PER 1,000 L.F.					\$18,830
TOTAL PER L.F.					\$18.83

REACH No. 3 ALTERNATE 1 TOTAL

REACH No. 3 ALTERNATE 1 TOTAL	LF	876	\$18.83	\$16,495
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ALTERNATE 2 - 42 FOOT WIDE MILL & PAVE TRAIL SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
411-03342	2" MILL DEPTH	SY	4088	\$3.22	\$13,163
304-06007	2" OVERLAY	TON	460	\$102.00	\$46,920
627-00005	RESTRIPE	LF	876	\$5.00	\$4,380
TOTAL PER 1,000 L.F.					\$73,588
TOTAL PER L.F.					\$73.59

REACH No. 3 ALTERNATE 2 TOTAL

REACH No. 3 ALTERNATE 2 TOTAL	LF	876	\$73.59	\$64,463
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ALTERNATE 3 - 10 FOOT WIDE REMOVE & CONCRETE PAVE SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00220	*REMOVE ASPHALT	SY	973.33	\$8.87	\$8,633
203-00000	*EXCAVATION	CY	107.10	\$32.13	\$3,441
304-06007	2" CLASS 6 ABC	CY	54.08	\$57.13	\$3,090
608-00028	6" CONCRETE PAVEMENT	SF	973.33	\$99.32	\$96,671
627-00005	RESTRIPE	LF	860	\$2.50	\$2,150
TOTAL PER 1,000 L.F.					\$130,120
TOTAL PER L.F.					\$130.12

REACH No. 3 ALTERNATE 3 TOTAL

REACH No. 3 ALTERNATE 3 TOTAL	LF	876	\$130.12	\$113,985
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* Asphalt assumed depth 4" / Excavation 4" depth

**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 4
(GUARDRAIL SECTION)**

PROTECTED ON-STREET 10' DESIGNATED TRAIL

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00220	*REMOVE ASPHALT	SY	1500	8.87	\$13,305
203-00000	*EXCAVATION	CY	165	32.13	\$5,301
304-06007	2" CLASS 6 ABC	CY	83.85	57.13	\$4,790
608-00028	6" CONCRETE PAVEMENT	SY	1500	99.32	\$148,980
627-00005	RESTRIPE	LF	1350	2.5	\$3,375
606-00710	NEW CONCRETE BARRIER	LF	1120	105	\$117,600
606-00720	USED CONCRETE BARRIER	LF	1348	72	\$97,056
514-00000	PIPE RAILING	LF	2563	30	\$76,890
606-01460	END ANCHORAGE	EA	2	8500	\$17,000
411-03342	6" SHOULDER PAVEMENT	TON	200	\$102.00	\$20,400
304-06007	8" SHOULDER CLASS 6 ABC	CY	198	\$57.13	\$11,312
TOTAL PER 1000 L.F.					\$382,229
TOTAL PER L.F.					\$382.23

REACH No. 4 GUARDRAIL SECTION TOTAL

REACH No. 4 TOTAL	LF	1350	\$382.23	\$516,010
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**LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 5
(GUARDRAIL SECTION to ASPEN STREET)**

ON-STREET DESIGNATED 4' BIKE LANE (X2)

ALTERNATE 1 - 4 FOOT WIDE MILL & PAVE TRAIL SECTION (X2)

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00240	2" MILL DEPTH	SY	2911.11	\$3.22	\$9,374
411-03342	2" OVERLAY	TON	327.57	\$102.00	\$33,412
627-00005	RESTRIPE	LF	3275	\$5.00	\$16,375
TOTAL PER 1000 L.F.					\$18,064
TOTAL PER L.F.					\$18.06

REACH No. 5 ALTERNATE 1 TOTAL

REACH No. 5 ALTERNATE 1 TOTAL	LF	3275	\$18.06	\$59,161
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ALTERNATE 2 - 32 FOOT WIDE MILL & PAVE TRAIL SECTION

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00240	2" MILL DEPTH	SY	11644.44	\$3.22	\$37,495
411-03342	2" OVERLAY	TON	1310.26	\$102.00	\$133,647
627-00005	RESTRIPE	LF	3275	\$5.00	\$16,375
TOTAL PER 1000 L.F.					\$57,257
TOTAL PER L.F.					\$57.26

REACH No. 5 ALTERNATE 2 TOTAL

REACH No. 5 ALTERNATE 2 TOTAL	LF	3275	\$57.26	\$187,517
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Cost Estimates (continued)

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN LA CROSSE AVENUE CONNECTOR TRAIL - REACH No. 5 (GUARDRAIL SECTION to ASPEN STREET)

ON-STREET DESIGNATED 4' BIKE LANE (X2)

ALTERNATE 3 - 4 FOOT WIDE REMOVE & CONCRETE PAVE SECTION (X2)

CDOT ITEM #	DESCRIPTION	UNIT	QUAN	COST	TOTAL
202-00220	*REMOVE ASPHALT	SY	2911.11	\$8.87	\$25,822
203-00000	*EXCAVATION	CY	320.22	\$32.13	\$10,289
304-06007	2" CLASS 6 ABC	CY	160.11	\$57.13	\$9,147
608-00028	6" CONCRETE PAVEMENT	SY	2911.11	\$99.32	\$289,131
627-00005	RESTRIPE	LF	3275	\$5.00	\$16,375
TOTAL PER 1,000 L.F.					\$107,103
TOTAL PER L.F.					\$107.10

REACH No. 5 ALTERNATE 3 TOTAL

REACH No. 5 ALTERNATE 3 TOTAL	LF	3275	\$107.10	\$350,764
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* Asphalt assumed depth 4" / Excavation 4" depth

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN SALT CREEK TRAIL - REACH No. 4 (PALO ALTO ST to BESSEMER DITCH)

LA CROSSE AVENUE TRAIL CONNECTOR SUMMARY - PREFERRED ALTERNATES

REACH No. 1 (ALTERNATE 2)				\$96,229
REACH No. 2				\$10,337
REACH No. 3 (ALTERNATE 2)				\$64,463
REACH No. 4				\$516,010
REACH No. 5 (ALTERNATE 2)				\$187,517
			TOTAL	\$874,556
Design Fee - 10%	LS	1	0.1	\$87,456
CM Materials Testing - 10%	LS	1	0.1	\$87,456
Traffic Control - 5%	LS	1	0.05	\$43,728
Erosion Control - 5%	LS	1	0.05	\$43,728
			SUBTOTAL	\$262,367
Contingency - 20%	LS	1	0.2	\$174,911
			TOTAL	\$1,311,833

**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
ARKANSAS TRAIL REACH No. 1 (MOFFAT STREET TRAILHEAD to LA CROSSE AVE)**

CONCRETE TRAIL TYPICAL SECTION (EXCESSIVE CLEARING) - 5736 L.F.

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	180	\$8.50	\$1,530
614-00011	CLASS 1 SIGNS	SF	100	\$23.88	\$2,388
608-0026	CONCRETE PAVING 6" TH'K.	SY	6373.33	\$65.00	\$414,266
201-00001	CLEAR & GRUB	AC	1.84	\$12,000.00	\$22,080
203-00000	UNCLASSIFIED EXCAVATION	CY	2974.22	\$32.13	\$95,562
707-54096	CHAIN LINK FENCE, 8' HT.	LF	4563	\$35.00	\$159,705
				TOTAL	\$695,531

CULVERT CROSSING (48"Ø x 20' TYP) (4)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
203-00000	UNCLASSIFIED EXCAVATION	CY	228.16	\$32.13	\$7,331
603-01485	48"Ø RCP (CIP)	LF	80	\$285.44	\$22,835
601-03000	CONCRETE CLASS D HEADWALL	CY	36.8	\$757.47	\$27,875
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	153.68	\$23.24	\$3,572
				TOTAL	\$61,612

RAILROAD UNDERCROSSING

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
201-00001	CLEAR & GRUB	AC	0.1	\$7,982.33	\$798
203-00000	UNCLASSIFIED EXCAVATION	CY	3220	\$32.13	\$103,459
506-00224	RIP RAP (24 IN)	CY	348	\$115.25	\$40,107
N/A	MISC. TRAIL COVER STRUCTURE	LS	1	\$250,000.00	\$250,000
				TOTAL	\$394,364

REACH No. 1 TOTAL

				TOTAL	\$1,151,507
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ARKANSAS RIVER TRAIL REACH No. 1 (PED BRIDGE No. 2)

ORIGINAL LOCATION (250 L.F. SPAN)

250' PEDESTRIAN BRIDGE COST - CUSTOM SINGLE SPAN BOX GIRDER

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
N/A	12'x250' BOX SECTION PED BRIDGE	SF	3000	\$405.58	\$1,216,740
601-03040	CONC CLASS D (BRIDGE)	CY	13.34	\$908.90	\$12,125
602-00020	REINFORCING STEEL (EPOXY)	LB	308.8	\$1.23	\$380
503-00024	DRILLED SHAFT 24"	LF	180	\$485.88	\$87,458
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	20	\$23.24	\$465
				TOTAL	\$1,317,168

**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
ARKANSAS RIVER TRAIL REACH No. 1 (PED BRIDGE No. 2)
(Continued)**

PROPOSED ALTERNATE LOCATION (150 L.F. SPAN)

150' PEDESTRIAN BRIDGE COST - PRE MANUFACTURED

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
N/A	12'x150' PEDESTRIAN BRIDGE	SF	1800	\$185.87	\$334,566
601-03540	CONC. CL DT (DECK TOPPING)	CY	11.11	\$1,100.00	\$12,221
601-03040	CONC CLASS D (BRIDGE)	CY	13.34	\$908.90	\$12,125
602-00020	REINFORCING STEEL (EPOXY)	LB	308.8	\$1.23	\$380
503-00018	DRILLED SHAFT 18"	LF	180	\$265.99	\$47,878
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	20	\$23.24	\$465
				TOTAL	\$407,635

ARKANSAS RIVER TRAIL REACH No. 2 (LA CROSSE AVE to ASPEN COURT)

CONCRETE TRAIL TYPICAL SECTION - 5020 L.F.

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	180	\$8.50	\$1,530
614-00011	CLASS 1 SIGNS	SF	100	\$23.88	\$2,388
608-0026	CONCRETE PAVING 6" TH'K.	SY	5577.78	\$65.00	\$362,556
201-00001	CLEAR & GRUB	AC	1.61	\$7,982.33	\$12,852
203-00000	UNCLASSIFIED EXCAVATION	CY	2602.96	\$32.13	\$83,633
707-54096	CHAIN LINK FENCE, 8' HT.	LF	4458	\$35.00	\$156,030
				TOTAL	\$462,958

CULVERT CROSSING (48"Ø x 20' TYP) (1)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
203-00000	UNCLASSIFIED EXCAVATION	CY	57.04	\$32.13	\$1,833
603-01485	48"Ø RCP (CIP)	LF	20	\$285.44	\$5,709
601-03000	CONCRETE CLASS D HEADWALL	CY	9.2	\$757.47	\$6,969
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	38.42	\$23.24	\$893
				TOTAL	\$15,403

REACH No. 2 TOTAL

				TOTAL	\$478,361
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ARKANSAS RIVER TRAIL REACH No. 2 (PED BRIDGE No. 2)

PROPOSED ALTERNATE LOCATION (250 L.F. SPAN)

250' PEDESTRIAN BRIDGE COST - CUSTOM SINGLE SPAN BOX GIRDER

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
N/A	12'x250' BOX SECTION PED BRIDGE	SF	3000	\$405.58	\$1,216,740

Cost Estimates (continued)

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN ARKANSAS RIVER TRAIL REACH No. 3 (ASPEN COURT TRAILHEAD SECTION)

CONCRETE TRAIL TYPICAL SECTION - 778 L.F.					
CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
608-0026	CONCRETE PAVING 6" TH'K.	SY	1383.11	\$65.00	\$89,902
201-00001	CLEAR & GRUB	AC	0.357	\$7,982.33	\$2,850
203-00000	UNCLASSIFIED EXCAVATION	CY	2595	\$32.13	\$83,377
404-03411	RETAINING WALL	SFF	7645	\$19.00	\$145,255
601-03000	CONCRETE CLASS D FOOTING	CY	195.5	\$853.00	\$166,762
604-00510	TYPE D INLET (10 FT)	EA	1	\$8,400.00	\$8,400
603-01245	24" RCP (CIP)	LF	1921	\$141.01	\$270,880
603-05024	24" RCES	EA	1	\$1,773.00	\$1,773
604-31010	MH BOX BASE (10 FT)	EA	4	\$8,600.00	\$34,400
506-00224	RIP RAP (24 IN)	CY	46.3	\$115.25	\$5,336
707-54096	CHAIN LINK FENCE, 8' HT.	LF	655	\$35.00	\$22,925
REACH No. 3 TOTAL				TOTAL	\$832,644

ARKANSAS RIVER TRAIL 250' BRIDGE ALTERNATIVE

REACH No. 1					\$1,151,507
REACH No. 2					\$478,361
REACH No. 3					\$832,644
250' PEDESTRIAN BRIDGE COST					\$1,317,168
				TOTAL	\$3,779,680
Design Fee - 10%	LS	1	0.1		\$377,968
CM Materials Testing - 10%	LS	1	0.1		\$377,968
Traffic Control - 5%	LS	1	0.05		\$188,984
Erosion Control - 5%	LS	1	0.05		\$188,984
				SUBTOTAL	\$4,913,584
Contingency - 20%	LS	1	0.2		\$755,936
				TOTAL	\$5,669,520

ARKANSAS RIVER TRAIL 150' BRIDGE ALTERNATIVE

REACH No. 1					\$1,151,507
REACH No. 2					\$478,361
REACH No. 3					\$832,644
150' PEDESTRIAN BRIDGE COST					\$407,635
				TOTAL	\$2,870,147
Design Fee - 10%	LS	1	0.1		\$287,015
CM Materials Testing - 10%	LS	1	0.1		\$287,015
Traffic Control - 5%	LS	1	0.05		\$143,507
Erosion Control - 5%	LS	1	0.05		\$143,507
				SUBTOTAL	\$3,731,191
Contingency - 20%	LS	1	0.2		\$574,029
				TOTAL	\$4,305,220

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN ASPEN CIRCLE/STREET (ARKANSAS TRAIL TO BESSEMER DITCH TRAIL)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
627-30410	PREFORM PVM'T MARKING	SF	72	\$10.97	\$790
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
				TOTAL PER 1000 L.F.	\$1,573
				TOTAL PER L.F.	\$1.57
ASPEN CIRCLE / STREET		LF	6840		\$10,762
Design Fee - 10%	LS	1	0.1		\$1,076
CM Materials Testing - 10%	LS	1	0.1		\$1,076
Traffic Control - 5%	LS	1	0.05		\$538
Erosion Control - 5%	LS	1	0.05		\$538
				SUBTOTAL	\$13,991
Contingency - 20%	LS	1	0.2		\$2,152
				TOTAL	\$16,143

**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
SALT CREEK TRAIL - REACH No. 1 (ARKANSAS RIVER to LA CROSSE AVE)**

10' TRAIL TYPICAL SECTION					
CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
608-0026	CONCRETE PAVING 6" TH'K.	SY	111.11	\$65.00	\$7,222
201-00001	CLEAR & GRUB	AC	0.037	\$7,982.33	\$295
203-00000	UNCLASSIFIED EXCAVATION	CY	51.85	\$32.13	\$1,666
TOTAL PER 100 L.F.					\$9,967

SALT CREEK TRAIL	TOTAL PER L.F. (CONC TRAILS)	\$99.67
REACH No. 1 CONCRETE ALTERNATIVE TOTAL		
	LF	500
		\$49,835

SALT CREEK TRAIL - REACH No. 2 (LA CROSSE AVE)

ON ROAD TYPICAL SECTION					
CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
627-30410	PREFORM PVM'T MARKING	SF	72	\$10.97	\$790
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
627-00005	EPOXY PAVEMENT MARKING	GAL	11	\$100.50	\$1,106
TOTAL PER 1000 L.F.					\$2,679
TOTAL PER L.F.					\$2.68

REACH No 2 (LA CROSSE AVE) TOTAL		
REACH No. 2 ON ROAD SECTION	LF	2430
		\$6,510

SALT CREEK TRAIL - REACH No. 3 (LA CROSSE AVE to PALO ALTO ST)

SHARED ROAD TYPICAL SECTION					
CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
627-30410	PREFORM PVM'T MARKING	SF	72	\$10.97	\$790
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
TOTAL PER 1000 L.F.					\$1,573
TOTAL PER L.F.					\$1.57

REACH No. 3 SHARED ROAD SECTION TOTAL		
REACH No. 3 SHARED ROAD SECTION TOTAL	LF	1120
		\$1,762

**PUEBLO RIVER TRAILS EXTENSION MASTER PLAN
SALT CREEK TRAIL - REACH No. 4 (PALO ALTO ST to BESSEMER DITCH)**

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
608-0026	CONCRETE PAVING 6" TH'K.	SY	111.11	\$65.00	\$7,222
201-00001	CLEAR & GRUB	AC	0.037	\$7,982.33	\$295
203-00000	UNCLASSIFIED EXCAVATION	CY	51.85	\$32.13	\$1,666
707-54096	CHAIN LINK FENCING, 8' HT.	LF	48	\$35.00	\$1,680
TOTAL PER 100 L.F.					\$11,647

SALT CREEK REACH No. 4	LF	4800
		\$559,058

CULVERT CROSSING (48"Ø x 20' TYP) (1)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
203-00000	UNCLASSIFIED EXCAVATION	CY	57.04	\$32.13	\$1,833
603-01485	48"Ø RCP (CIP)	LF	20	\$285.44	\$5,709
601-03000	CONCRETE CLASS D HEADWALL	CY	9.2	\$757.47	\$6,969
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	38.42	\$23.24	\$893
TOTAL					\$15,403

ADDITIONAL GRADING @ PALO ALTO STREET

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
203-00000	UNCLASSIFIED EXCAVATION	CY	651.85	\$32.13	\$20,944
TOTAL					\$20,944

REACH No. 4

CONCRETE TRAIL					\$559,058
CULVERT CROSSING					\$15,403
ADDITIONAL GRADING					\$20,944
TOTAL					\$595,405

SALT CREEK TRAIL SUMMARY

REACH No. 1					\$49,835
REACH No. 2					\$6,510
REACH No. 3					\$1,762
REACH No. 4					\$595,405
TOTAL					\$653,512

Design Fee - 10%	LS	1	0.1	\$65,351	
CM Materials Testing - 10%	LS	1	0.1	\$65,351	
Traffic Control - 5%	LS	1	0.05	\$32,676	
Erosion Control - 5%	LS	1	0.05	\$32,676	
SUBTOTAL					\$849,566

Contingency - 20%	LS	1	0.2	\$130,702	
TOTAL					\$980,268

Cost Estimates (continued)

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN BESSEMER DITCH (SALT CREEK TRAIL to WILLBANKS PROPERTY)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
608-0026	CONCRETE PAVING 6" TH'K.	SY	111.11	\$65.00	\$7,222
201-00001	CLEAR & GRUB	AC	0.037	\$7,982.33	\$295
203-00000	UNCLASSIFIED EXCAVATION	CY	51.85	\$32.13	\$1,666
707-54096	CHAIN LINK FENCING, 6' HT.	LF	202	\$25.00	\$5,050
TOTAL PER 100 L.F.					\$15,017
TOTAL PER L.F. (CONC TRAILS)					\$150.17

BESSEMER DITCH TRAIL	LF	13050			\$1,959,723
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40' PEDESTRIAN BRIDGE COST

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
N/A	12'x40' PEDESTRIAN BRIDGE	SF	480	\$185.87	\$89,218
601-03540	CONC. CL DT (DECK TOPPING)	CY	2.964	\$1,100.00	\$3,260
601-03040	CONC CLASS D (BRIDGE)	CY	13.34	\$908.90	\$12,125
602-00020	REINFORCING STEEL (EPOXY)	LB	308.8	\$1.23	\$380
503-00018	DRILLED SHAFT 18"	LF	96	\$265.99	\$25,535
206-00200	STRUCTURAL BACKFILL (CLASS 2)	CY	20	\$23.24	\$465
				TOTAL	\$130,982
TOTAL BRIDGES (4)					\$523,930

BESSEMER DITCH TRAIL SUMMARY

BESSEMER DITCH TRAIL	LF	13050			\$1,959,723
40' PEDESTRIAN BRIDGE COST	EA	4			\$523,930
					\$2,483,653
Design Fee - 10%	LS	1	0.1	\$248,365	
CM Materials Testing - 10%	LS	1	0.1	\$248,365	
Traffic Control - 5%	LS	1	0.05	\$124,183	
Erosion Control - 5%	LS	1	0.05	\$124,183	
				SUBTOTAL	\$3,228,749
Contingency - 20%	LS	1	0.2	\$496,731	
				TOTAL	\$3,725,479

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN COUNTY FARM ROAD (ASPEN STREET to 30th LANE)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
627-30410	PREFORM PVM'T MARKING	SF	72	\$10.97	\$790
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
TOTAL PER 1000 L.F.					\$1,573
TOTAL PER L.F.					\$1.57

COUNTY FARM ROAD	LF	20555			\$32,342
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Design Fee - 10%	LS	1	0.1	\$3,234	
CM Materials Testing - 10%	LS	1	0.1	\$3,234	
Traffic Control - 5%	LS	1	0.05	\$1,617	
Erosion Control - 5%	LS	1	0.05	\$1,617	
				SUBTOTAL	\$42,045
Contingency - 20%	LS	1	0.2	\$6,468	
				TOTAL	\$48,513

PUEBLO RIVER TRAILS EXTENSION MASTER PLAN EVERETT ROAD (23rd LANE to 30th LANE)

CDOT ITEM No.	DESCRIPTION	UNIT	QUAN	COST	TOTAL
627-30410	PREFORM PVM'T MARKING	SF	72	\$10.97	\$790
614-00216	POST 2x2 "T"	LF	36	\$8.50	\$306
614-00011	CLASS 1 SIGNS	SF	20	\$23.88	\$478
TOTAL PER 1000 L.F.					\$1,573
TOTAL PER L.F.					\$1.57

EVERETT ROAD	LF	10550			\$16,600
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Design Fee - 10%	LS	1	0.1	\$1,660	
CM Materials Testing - 10%	LS	1	0.1	\$1,660	
Traffic Control - 5%	LS	1	0.05	\$830	
Erosion Control - 5%	LS	1	0.05	\$830	
				SUBTOTAL	\$21,580
Contingency - 20%	LS	1	0.2	\$3,320	
				TOTAL	\$24,900

Trail Maintenance Data Sources

Pueblo River Trails Extension Master Plan Update
 Comparative Annual Trails Maintenance Budgeting
 Prepared: September 25, 2020

Municipality / District	Response	Other Info / Notes																		
City of Pueblo, CO	Trails Maintenance included under Parks & Open Space Budget																			
City of Westminster, CO	Trails Maintenance included under Parks & Open Space Budget																			
South Suburban Parks & Recreation District	Average LF maintenance Costs: Crusher Fines: \$0.88 (grooming as necessary, weed control, and 1/2" overlay every 5 years) Asphalt: \$2.04 (sweeping, weed control, crack filling as necessary and amortized replacement cost every 25 years) Concrete: \$4.52 (sweeping, weed control, raised edge grinding as necessary and amortized replacement cost every 50 years)	From 2020 Budget Published Online, Total Trail Maintenance Expenditures = \$854,315 With 101.4 miles of trail, = \$8,425 per mile OR If Salaries, Overtime, and Fringe Benefits are not included (leaving only Minor Tools & Equipment & Ser/Mat for Paths, Roads, Parking Lots) With 101.4 miles of trail, = \$3,150 per mile																		
Rails-to-Trails		2015 Study of 2014 Maintenance Costs for many different municipalities and districts: Table 1. Estimated Costs Per Mile <table border="1"> <thead> <tr> <th>Source</th> <th>Asphalt Surface</th> <th>Non-Asphalt Surface</th> </tr> </thead> <tbody> <tr> <td>RTC Maintenance & Operations Report - 2014</td> <td>\$1,971/mile</td> <td>\$1,006/mile</td> </tr> <tr> <td>RTC Maintenance & Operations 2004 Report</td> <td>\$1,458/mile</td> <td>\$1,478/mile</td> </tr> </tbody> </table>	Source	Asphalt Surface	Non-Asphalt Surface	RTC Maintenance & Operations Report - 2014	\$1,971/mile	\$1,006/mile	RTC Maintenance & Operations 2004 Report	\$1,458/mile	\$1,478/mile									
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Mile High Flood District	\$5 LF annually for path maintenance (\$26,400/mile)	For Maintenance Paths																		
Americantrails.org	varies from \$1,200/mile to \$2,525/mile	2007 Study, Milwaukee Dept. of Parks & Rec; asphalt or crushed gravel trails; does not include snow removal (additional \$24.13/mile to \$154.13/mile)																		
Jefferson County Open Space	Awaiting Response																			
East Bay Regional Park District (Oakland, CA)	Awaiting Response																			
El Paso County	Average \$1,200/mile	Includes Basic Maintenance - box blading and minor addition of trail surfacing																		
City of Colorado Springs	Awaiting Jason Alwine Response																			
Ohio River Greenway (November 2014)		Table 2. Assorted Maintenance Cost Estimates <table border="1"> <thead> <tr> <th>Source</th> <th>Trail</th> <th>Cost-per-mile</th> </tr> </thead> <tbody> <tr> <td>provided in the Iowa Trails 2000 plan by the Iowa Department of Transportation</td> <td>a mixture of different surfaces</td> <td>\$1,500 per mile</td> </tr> <tr> <td>Milwaukee County Park System</td> <td>all asphalt paths</td> <td>\$2,525 per mile</td> </tr> <tr> <td>Rail Trail Maintenance & Operation Manual provided by the Rails-to-Trails Conservancy</td> <td></td> <td>\$1,200 per mile (absolute minimum)</td> </tr> <tr> <td>for government run trails in the Rail Trail Maintenance & Operation Manual provided by the Rails-to-Trails Conservancy</td> <td></td> <td>\$2,077 per mile</td> </tr> <tr> <td>in the Trail Cost Model - Draft by the Wisconsin Department of Natural Resources</td> <td>unpaved trail</td> <td>\$2,042.06 per mile</td> </tr> </tbody> </table> Source: Milwaukee County Dept. of Parks, Recreation, and Culture http://www.americantrails.org/resources/ManageMaintain/MilwMaintcost.html	Source	Trail	Cost-per-mile	provided in the Iowa Trails 2000 plan by the Iowa Department of Transportation	a mixture of different surfaces	\$1,500 per mile	Milwaukee County Park System	all asphalt paths	\$2,525 per mile	Rail Trail Maintenance & Operation Manual provided by the Rails-to-Trails Conservancy		\$1,200 per mile (absolute minimum)	for government run trails in the Rail Trail Maintenance & Operation Manual provided by the Rails-to-Trails Conservancy		\$2,077 per mile	in the Trail Cost Model - Draft by the Wisconsin Department of Natural Resources	unpaved trail	\$2,042.06 per mile
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Arapahoe County Open Space	Awaiting Response																			
City of Aurora	Trails Maintenance included under Parks & Open Space Budget	No contact, but published budget does not break out maintenance from Open Space budget																		
City of Thornton	No asset management system in place to provide long term maintenance costs	For reference, current replacement cost estimates are: Concrete Trails: \$6.50 /SF, 6" depth, 8' wide Soft Path Decomposed Granite: \$3.50/SF, 4" depth																		
City & County of Broomfield	\$1 / LF	Trail maintenance including snow removal																		
City of Boulder (Gerrit Slatter)	Awaiting Response																			
CDOT - Betsy Jacobsen	No relevant information as CDOT trails are maintained by separate jurisdictions	Directed us to Fletcher Jacobs with Department of Natural Resources																		
Department of Natural Resources - Fletcher Jacobs	Trails Maintenance typically pulled from operating budgets and vary widely across different agencies, depending on standard of maintenance																			
City & County of Denver	\$2,555/mile for High Line Canal Trail in 2019, 8' width asphalt \$747/mile for High Line Canal Trail in 2019, 8' width asphalt \$121/mile for High Line Canal Trail in 2019, 8' width asphalt	includes trail maintenance and maintenance of surrounding amenities (irrigation, tree & grass maintenance, graffiti removal, etc.) maintenance of trail only, including snow removal maintenance of trail only, NOT including snow removal																		
City of Fort Collins	Awaiting Response																			

Environmental

ENVIRONMENTAL CONDITIONS SUMMARY

The purpose of the following summary is to identify known environmental conditions throughout the potential trail corridor areas, based on review of existing and available information. The intent of the summary is to identify potential known environmental hazard areas to be aware of and to avoid. The following environmental "scan" has been prepared to identify potential site environmental conditions to guide the planning and design of the project.

Site Inventory

A comprehensive inventory of existing site conditions for the overall project area was prepared using GIS data provided by the Pueblo County GIS Department, Colorado Parks and Wildlife, the Environmental Protection Agency, the US Fish and Wildlife Service, and the Federal Emergency Management Agency. Types of data gathered and evaluated included land use, ownership, utilities, trails, transit routes, railroads, parking, parks, streams and floodplains, vegetation and wetlands, endangered species and habitat, cultural and historical features, environmental concerns, and topography.

Water Resources and Floodplains

Good maintenance begins with good planning. Surface waters within the project area include the Arkansas River, Salt Creek and the Bessemer Ditch, as documented in the project Site Inventory mapping.

The Arkansas River FEMA 100-year floodplain is documented in project Site Inventory mapping used for project master planning. Based on anticipated impacts to FEMA floodplain, and necessary design and permitting requirements, no near-term improvements within the Arkansas River floodplain are proposed in this Master Plan Update. Preliminary design studies have been prepared and provided to understand potential impacts and explore their potential mitigation, for future long-range planning. The area within

FEMA 100-year floodplain is subject to FEMA FIRM regulations for proposed improvements which are not allowed to impact established floodplain limits.

The Salt Creek FEMA 100-year floodplain is documented in project Site Inventory mapping, used for project master planning. The course of Salt Creek adjacent to proposed On-Street Trail improvements is separated by a steep embankment, with FEMA floodplain limits very near the top of that embankment. Proposed Master Plan improvements, as part of the LaCrosse Connector Trail have been evaluated so no fill or modification to the embankment is proposed. Master Plan studies for those improvements are based on no modification beyond the existing pavement edge, adjacent to the creek.

Where Salt Creek passes under LaCrosse Ave. through an existing culvert, the FEMA 100-year floodplain over-tops the roadway, impacting a roadway segment of approximately 925 linear feet in roadway length. The LaCrosse Connector Trail is proposed through this segment as an On-Street Trail, located within the limits of the existing roadway pavement, with protective concrete safety barriers protecting trail users from roadway traffic. To prevent any impact to the existing 100-year FEMA floodplain limits, trail pavement improvements must be designed so there is no rise in pavement elevations, and protective concrete barriers must be designed to provide a raised gap to allow 100-year flood flows to pass under the barriers, to avoid impeding flood flows.

Because Salt Creek flows through EVRAZ Rocky Mountain Steel (EVRAZ RMS) property, south of Santa Fe Drive (US 50), that is regulated by an environmental covenant, run-off from Salt Creek is subject to (settling pond) treatment prior to release into the Arkansas River. Specifically, Surface water conveyed through the EVRAZ RMS industrial wastewater system will be managed in accordance with the EVRAZ

RMS National Pollution Discharge Elimination System Permit No. CO 0000621 issued March 24, 2011 or subsequent Permit revisions.

The Bessemer Ditch is a regulated flow, agricultural irrigation canal that is operated using the practice of seasonal "water storage", with water delivery occurring over an eight (8) month period, with the ditch diversion (at the Pueblo Reservoir) closed for a water storage period the other four months of the year for the next irrigation season. Due to its regulated flow, it has no designated FEMA Floodplain.

The Bessemer Irrigating Ditch Company was incorporated in 1888 to deliver irrigation water diverted from the Arkansas River for agricultural crops along its 34 miles, from what is now the Pueblo Reservoir, to (approximately) Avondale, CO. The Bessemer Ditch Company may or may not own the property upon which it is located. In Colorado, irrigation ditches are granted the right of way to deliver water for irrigation in the State of Colorado Constitution, regardless of property ownership. The Bessemer Ditch is located within a 100' right-of-way (ROW) measured 50' in each direction from its centerline.

The Bessemer ditch transmits water by gravity flow at a rate of 140 Cubic Feet per Second (CFS), with 85 head gates along its course that are manually opened and closed to deliver water to the properties it serves. At that flow rate, the water moves through the ditch at approximately one (1) mile per hour (MPH). In the project area (St. Charles Mesa), there are 20-25 manually operated head gates delivering irrigation water to users via lateral canals across the mesa.

Wetlands

No Wetland delineation has been prepared at this early phase of master planning work. As individual projects are proposed for design and implementation, additional study and wetland delineations may be warranted for permitting. Because the overwhelming majority of the

project area is in upland, urbanized areas, no wetland impacts are anticipated for proposed On-Street Trails corridors. Arkansas River Trail segments may encounter wetland areas that require further study and potential wetland delineation under Section 404 of the Clean Water Act. Those potential studies would occur at the design and implementation phases of work for permitting.

Environmental Hazards

On March 3, 2015, EVRAZ Rocky Mountain Steel (EVRAZ RMS) granted an Environmental Covenant to the Colorado Department of Public Health and Environment pursuant to the Colorado Hazardous Waste Act 25-15-101, restricting the use of EVRAZ RMS subject properties to prohibit residential or public use of the properties and groundwater beneath the properties.

The Covenant does not prevent EVRAZ RMS from continuing existing uses, or expanding those uses consistent with the Materials Management Plan. The most recent approved version of the Materials Management Plan is on file at the Colorado Department of Health and Environment, Hazardous Materials and Waste Management Division, Record Center and may be obtained from the Department at the following address.

Mr. Charles Adams
Environmental Protection Specialist
HMWMD-SHWP-B2
Hazardous Waste and Materials
Management Division
Colorado Department of Public Health and
Environment
4300 Cherry Creek Drive South
Denver, Colorado 80246-1530

The Covenant lists ten (10) requirements. The first, Use Restrictions (summarized below), is most directly related to potential trail routing on EVRAZ RMS properties.

1. Use Restrictions:
 - a. Prohibition on Residential or Public Use ... The OWNER shall not allow any public use, including but not limited to, playgrounds, parks, schools, daycare centers (whether independent or ancillary to a permitted use), community centers, hospitals or adult care centers...
 - b. Prohibition on Agricultural Use
 - c. Prohibition on Groundwater Use
 - d. Prohibition on Soil Disturbance ... must be done in accordance with the most recent approved version of the Materials Management Plan...
 - e. Prohibition on Sediment Disturbance
 - f. Prohibition on Surface Water Use
 - g. Prohibition on interfering with Permit Activities

Vegetation

Existing woody vegetation along the Arkansas River generally consists of riparian trees and shrubs and other 'volunteer' non-native species. As individual projects are proposed for design and implementation, additional study may be warranted for permitting.

Threatened and Endangered Species

At the master planning phase of work, no biological inventory studies have been prepared. As individual projects are proposed for design and implementation, additional study may be warranted for permitting. More specifically, the Utes Lady's Tresses orchid (*Spiranthes diluvialis*), and Preble's Meadow Jumping Mouse (*Zapus hudsonius preblei*) habitat includes lowland and areas subject to flooding, which may include areas along the Arkansas River Trail.

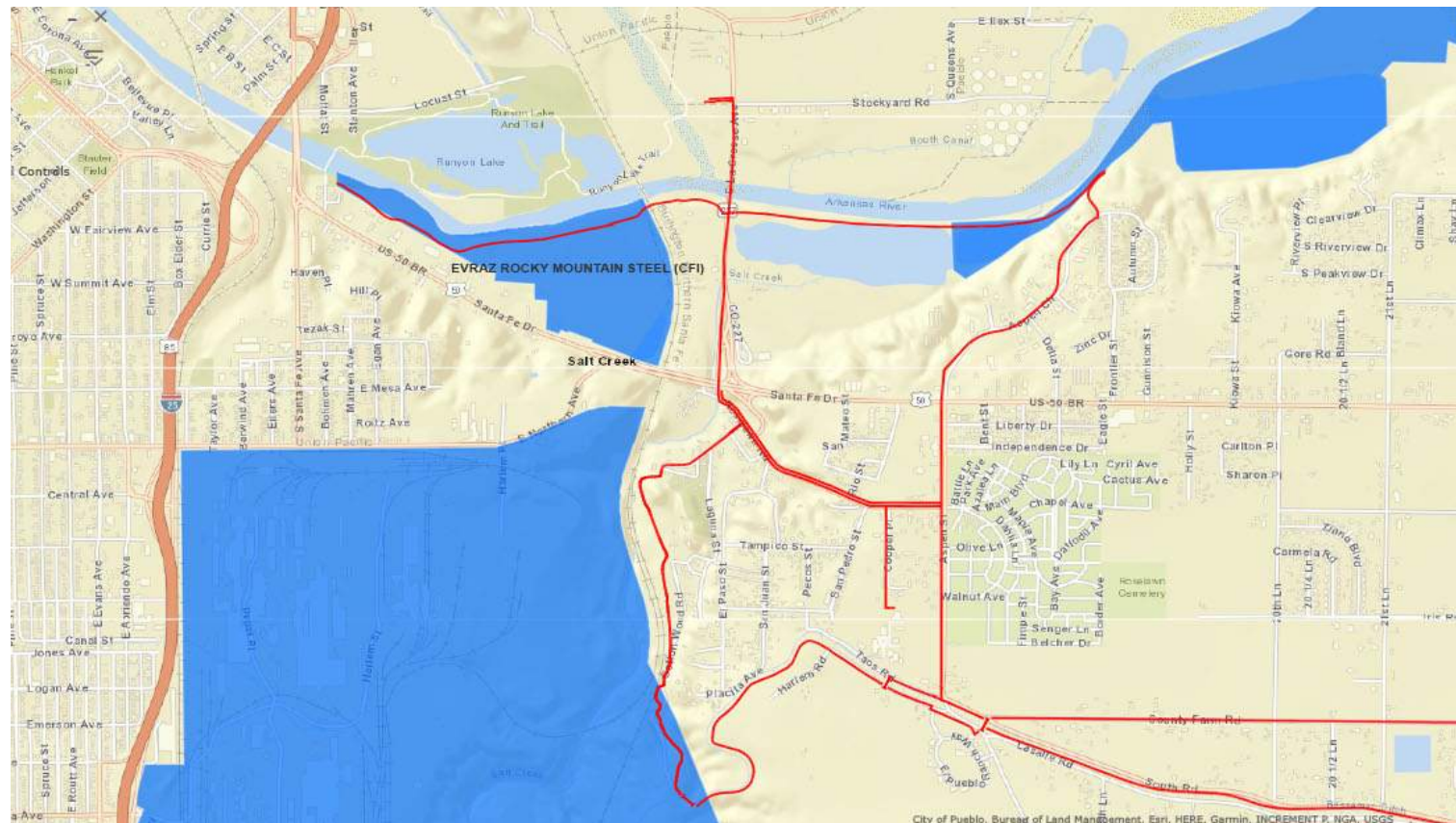
Historic Properties

A long-range, On-Street Trail route is proposed connecting through the historic settlement

known as Salt Creek, Colorado, as a segment proposed as the Salt Creek Trail. Proposed improvements are limited to On-street "sharrow" signage and pavement markings.

Other Potential Environmental Issues

At this master planning phase of work, no other potential environmental impacts have been identified. As individual projects are proposed for design and implementation, additional study may be warranted for permitting that address Noise Levels, Visual Resources, Cumulative Impacts or potential Recreational Impacts.



Environmental (continued)

1939418 ENV_COVT 04/09/2013 02:56:18 PM
Page: 1 of 14 R 91.00 D 0.00 T 91.00
Gilbert Ortiz Clerk/Recorder, Pueblo County, Co



This property is subject to an Environmental Covenant held by the Colorado Department of Public Health and Environment pursuant to section 25-15-321, C.R.S.

ENVIRONMENTAL COVENANT

CF&I, L.P., d/b/a Evraz Rocky Mountain Steel ("Evraz RMS") grants an Environmental Covenant ("Covenant") this 5 day of March, 2013 to the Hazardous Materials and Waste Management Division of the Colorado Department of Public Health and the Environment ("the Department") pursuant to § 25-15-321 of the Colorado Hazardous Waste Act, § 25-15-101, *et seq.* The Department's address is 4300 Cherry Creek Drive South, Denver, Colorado 80246-1530.

WHEREAS, Evraz - Pueblo, a Division of Evraz Inc. NA, is the owner of certain property commonly referred to as Evraz Rocky Mountain Steel, located at 2100 South Freeway, Pueblo, CO, 81004, more particularly described in Attachment A, attached hereto and incorporated herein by reference as though fully set forth (hereinafter referred to as "the Property"); and

WHEREAS, pursuant to Resource Conservation & Recovery Act Permit # CO-05-09-29-01, the Property is the subject of enforcement and remedial action pursuant to the Resource Conservation and Recovery Act, 42 U.S.C. § § 6926, *et seq.* ("RCRA"); and

WHEREAS, the purpose of this Covenant is to ensure protection of human health and the environment by restricting the use of the Property to prohibit residential or public use of the property and the groundwater beneath the Property; and

WHEREAS, Evraz RMS desires to subject the Property to certain covenants and restrictions as provided in Article 15 of Title 25, Colorado Revised Statutes, which covenants and restrictions shall burden the Property and bind Evraz RMS and all parties having any right, title or interest in the Property, or any part thereof, their heirs, successors and assigns, and any persons using the land, as described herein, for the benefit of the Department and Evraz RMS; and

WHEREAS, notwithstanding anything in this Covenant to the contrary, nothing in this Covenant shall be construed as preventing or prohibiting Evraz RMS from continuing the existing uses on the Property, or from expanding those uses consistent with the Materials Management Plan, incorporated herein by reference as though fully set forth and hereinafter referred to as "the Materials Management Plan".

NOW, THEREFORE, Evraz RMS hereby grants this Covenant to the Department, and declares that the Property as described in Attachment A shall hereinafter be bound by, held, sold, and conveyed subject to the following requirements set forth in paragraphs 1 through 10, below, which shall run with the Property in perpetuity and be binding on Evraz

1939418 ENV_COVT 04/09/2013 02:56:18 PM
Page: 2 of 14 R 91.00 D 0.00 T 91.00
Gilbert Ortiz Clerk/Recorder, Pueblo County, Co



RMS and all parties now or subsequently having any right, title or interest in the Property, or any part thereof, their heirs, successors and assigns, and any persons using the land, as described herein, except as otherwise provided in C.R.S. § 25-15-318. As used in this Covenant, the term OWNER means the then record owner of the Property and, if any, any other person or entity otherwise legally authorized to make decisions regarding the transfer of the Property or placement of encumbrances on the Property, other than by the exercise of eminent domain.

1) Use restrictions

- a) Prohibition on Residential or Public Use. The OWNER shall not allow residential use, including, but not limited to, any single family or multi-family residential dwelling or living unit, whether permanent or temporary. The OWNER shall not allow any public use, including but not limited to, playgrounds, parks, schools, daycare centers (whether independent or ancillary to a permitted use), community centers, hospitals, or adult care centers on the Property.
- b) Prohibition on Agricultural Use. The OWNER shall not allow or permit any agricultural use on the Property, including but not limited to, the cultivation or storage of any crop or the grazing, feeding or keeping of any animal for agricultural or commercial purposes.
- c) Prohibition on Groundwater Use. Groundwater from within areas shown in Figures 1, 2, and 3 shall not be removed by well or other means for domestic, agricultural, commercial or other consumptive uses. This limitation shall not apply to existing wells or wells used for groundwater monitoring or groundwater remediation purposes. This restriction also does not preclude groundwater extraction/management arising from construction dewatering which is conducted in accordance with the Colorado Water Quality Control Act (§ 25-8-101, *et seq.*).
- d) Prohibition on Soil Disturbing Activities. Activities resulting in the removal or disturbance of soil from areas of the facility shown in Figures 1, 2 and 3 must be done in accordance with the most recent approved version of the Materials Management Plan.

[The most recent approved version of the Materials Management Plan is on file at the Colorado Department of Public Health and Environment, Hazardous Materials and Waste Management Division, Record Center and may be obtained from the Department at the address provided in Section 11 herein.]

- e) Prohibition on Sediment Disturbance. Dredging, excavation, grading, digging, filling in, covering or any sort of disturbance of sediments in areas of the facility shown in Figures 1, 2, and 3, must be done in accordance with the Materials Management Plan or EVRAZ RMS Standard Operating Procedures.
- f) Prohibition on Surface Water Use. Surface water from within areas shown in Figures 1, 2, and 3 shall not be used for domestic, agricultural, commercial or other consumptive use without modification of this Covenant pursuant to paragraph 3. Water contained within the EVRAZ RMS industrial water conveyance system and the Bessemer Ditch are excluded from this prohibition. Surface water conveyed through the EVRAZ RMS industrial wastewater system will be managed in accordance with the EVRAZ RMS National Pollution Discharge Elimination System Permit No CO0000621 issued March 24, 2011 or subsequent Permit revisions.
- g) Prohibition on Interfering With Permit Activities. No activity shall be conducted that violates or interferes with the activities specified in the October 30, 2005, Post-Closure Permit, or subsequent Post-Closure Permit revisions, the most recent approved revisions of which are on file at the Colorado Department of Public Health and Environment, Hazardous Materials and Waste Management Division, Record Center and may be obtained from the Department at the address provided in Section 11 herein.
- 2) Modifications. This Covenant runs with the land and is perpetual, unless modified or terminated pursuant to this paragraph. The OWNER may request that the Department approve a modification or termination of the Covenant. The request shall contain information showing that the proposed modification or termination shall, if implemented, ensure protection of human health and the environment. The Department shall review any submitted information, and may request additional information. If the Department determines that the proposal to modify or terminate the Covenant will ensure protection of human health and the environment, it shall approve the proposal. No modification or termination of this Covenant shall be effective unless the Department has approved such modification or termination in writing. The Department shall provide a response to the proposal to modify or terminate the Covenant within sixty (60) days. Information to support a request for modification or termination may include one or more of the following:
- a) a proposal to perform additional remedial work;
 - b) new information regarding the risks posed by the residual contamination;
 - c) information demonstrating that residual contamination has diminished;
 - d) information demonstrating that an engineered feature or structure is no longer necessary;
 - e) information demonstrating that the proposed modification would not adversely impact the remedy and is protective of human health and the environment; and

- f) other appropriate supporting information.
- 3) Effect of Covenant; Merger. This Covenant supersedes and replaces all other covenants which may be in effect at the Property as of the date of this Covenant. Further, notwithstanding anything in this Covenant to the contrary, should the OWNER grant another environmental covenant to the Department, which covenant should encumber the Property, or any portion thereof, encumbered by this Covenant, the Department agrees that this Covenant and any subsequent covenants shall merge, and this Covenant shall be incorporated into any new covenant, and they shall be treated as if there is only one covenant for the Property.
- 4) Conveyances. The OWNER shall notify the Department at least fifteen (15) days in advance of any proposed grant, transfer or conveyance of any interest in any or all of the Property.
- 5) Notice to Lessees. The OWNER agrees to incorporate either in full or by reference the restrictions of this Covenant in any future leases, licenses, or other instruments granting a right to use the Property. The OWNER agrees to send notice of this Covenant to the holders of record of any existing leases, licenses, rights-of-way, easements or other instruments granting a right to use the Property ("Holders"). The OWNER is under no obligation to provide consideration to the Holders in exchange for an agreement to subject the Holders' property interests to this Covenant, nor is the OWNER required to seek modification by a court of competent jurisdiction of any leases, licenses, easements or other instruments granting a right to use the Property. OWNER shall not be held liable by the Department for a violation of this Covenant by any of the Holders.
- 6) Notification for proposed construction and land use. The OWNER shall notify the Department when submitting an application to a local government for a building permit or change in land use.
- 7) Third Party Beneficiary. The OWNER of the property is a third party beneficiary with the right to enforce the provisions of the Covenant as provided in § 25-15-322, C.R.S.
- 8) Inspections. The Department shall have the right of entry to the Property at reasonable times with prior notice for the purpose of determining compliance with the terms of this Covenant. Nothing in this Covenant shall impair any other authority the Department may otherwise have to enter and inspect the Property.
- 9) No Liability. The Department does not acquire any liability under State law by virtue of accepting this Covenant.
- 10) Enforcement. The Department may enforce the terms of this Covenant pursuant to § 25-15-322, C.R.S. against the person or entity believed or alleged to be performing or conducting an activity on the Property in violation of this Covenant. The OWNER may

Accepted by the Colorado Department of Public Health and Environment this 12th day of March, 2013

By: Gary W. Baughman
Title: Director, HAWARD

STATE OF COLORADO)
) ss:
COUNTY OF DENVER)

The foregoing instrument was acknowledged before me this 12 day of MARCH, 2013 by GARY BAUGHMAN on behalf of the Colorado Department of Public Health and Environment.

Claudette M. Junis
Notary Public
4300 Cherry Creek Dr So
Address
Denver, CO 80246

My commission expires: October 21, 2015



LEGAL DESCRIPTION:

A tract of land located in the East Half and the Northwest Quarter of Section 6, Township 21 South, Range 64 West of the 6th P.M. in Pueblo County, Colorado more particularly described as follows;

Commencing from the Center of said Section 6, also being the Point of Beginning; Thence 40 feet ± North along the North-South Center line of said Section 6 and the East line of a parcel of land recorded at Reception No. 1893835 of the Pueblo County records, to a point on the North line of said parcel; Thence N60°05'51"W, a distance of 191 feet ±, along said North line to a point being the Northwest corner of said parcel; Thence S29°59'54"W, a distance of 30 feet ± along the West line of said parcel to a point being the Northeast Corner of Bullen's Subdivision; Thence N60°00'06"W, a distance of 1879 feet ± to a point on the centerline of Stanton Street; Thence continuing North along said Stanton St., a distance of 178 feet ± to the South bank of the Arkansas River; Thence Easterly along said South bank of the Arkansas River to a point on the West line of the BNSF Railroad right of way; Thence S24°36'47"E, a distance of 676.9 feet ± along said West right of way line; Thence continuing along said West right of way on a curve to the right with a radius of 1854 feet ± and a length of 894.52 feet ±; Thence continuing along West right of way S14°31'48"W, a distance of 543.2 feet ± to a point on the North right of way line of U.S. HWY 50;

Thence along said U.S. HWY 50 the following 12 courses:
Thence N76°35'58"W, a distance of 126 feet ±;
Thence on a curve to the right with a radius of 2740 feet ± and an arc length of 341.14 feet ±;
Thence N68°54'10"W, a distance of 358.3 feet;
Thence N21°03'01"E, a distance of 10.0 feet;
Thence N69°00'37"W, a distance of 279.9 feet;
Thence S21°03'33"W, a distance of 25.0 feet;
Thence N68°55'47"W, a distance of 89.2 feet;
Thence S22°21'21"W, a distance of 45.0 feet;
Thence S20°32'02"W, a distance of 10.0 feet;
Thence N68°39'08"W, a distance of 139.7 feet;
Thence S21°18'36"W, a distance of 10.0 feet;
Thence N73°45'30"W, a distance of 13.5 feet± to a point on the East line of a parcel recorded in Book 2769 Pg. 913 of the Pueblo County records;
Thence North, a distance of 357.4 feet± along said parcel to the Northeast corner of said parcel; Thence West, a distance of 141 feet± along said North line of said parcel to a point, being the East line of a parcel recorded at Reception No. 1893835 of the Pueblo County records; Thence North along said East line a distance of 191 feet± to the Northeast corner of said parcel; Thence N85°57'W, a distance of 455.6 feet± to a point on the North-South Centerline of said Section 6; Thence North along said centerline, a distance of 51 feet± to the Center of Section 6 being the Point of Beginning.

The above described parcel contains 73± Acres.

And

A parcel of land located in a portion of Section 4 and 5, Township 21 South, Range 64 West, of the 6th P.M., and a portion of Section 33, Township 20 South, Range 64 West, of the 6th P.M., County of Pueblo, State of Colorado, being more particularly described as follows:

Environmental (continued)

1939418 ENV COVT 04/09/2013 02:56:18 PM
Page: 9 of 14 R 91.00 D 0.00 T 91.00
Gilbert Ortiz Clerk/Recorder, Pueblo County, Co



Commencing at the Southwest corner of Section 5, Township 21 South, Range 64 West; thence N 37°42'49" E, a distance of 4473.51 feet, to the Point of Beginning (the Basis of Bearings for the herein described parcel is N 00°23'47" E, along the West line of the northeast ¼ of Section 8 Township 21 South, Range 64 West); thence S 02° 23' 03" E, a distance of 606.49 feet; thence S 53° 39' 25" E, a distance of 178.42 feet; thence N 56° 56' 30" E, a distance of 516.82 feet; thence N 51° 32' 03" E, a distance of 312.95 feet; thence N 60° 57' 32" E, a distance of 341.67 feet; thence N 48° 48' 04" E, a distance of 348.82 feet; thence N 41° 31' 37" E, a distance of 760.96 feet; thence N 50° 51' 01" E, a distance of 322.92 feet; thence N 40° 04' 14" E, a distance of 376.10 feet; thence N 84° 26' 18" E, a distance of 206.85 feet; thence S 07° 02' 10" E, a distance of 516.04 feet; thence S 38° 26' 17" E, a distance of 140.22 feet; thence N 88° 00' 23" E, a distance of 1352.45 feet; thence N 74° 33' 02" E, a distance of 682.61 feet; thence N 50° 52' 50" E, a distance of 545.24 feet; thence N 00° 54' 46" W, a distance of 595.17 feet; N 53° 54' 20" W, a distance of 79.66 feet; thence N 41° 50' 43" E, a distance of 211.41 feet; thence S 88° 30' 12" E, a distance of 632.45 feet; thence N 06° 06' 05" E, a distance of 277.78 feet; thence S 69° 06' 25" E, a distance of 209.02 feet; thence N 04° 27' 19" E, a distance of 450.93 feet; thence N 42° 15' 38" W, a distance of 764.26 feet; thence S 88° 06' 17" W, a distance of 344.94 feet; thence S 60° 31' 09" W, a distance of 216.09 feet; thence S 53° 32' 40" W, a distance of 332.51 feet; thence S 72° 20' 37" W, a distance of 432.12 feet; thence S 54° 08' 34" W, a distance of 508.15 feet; thence S 66° 46' 50" W, a distance of 1519.32 feet; thence S 57° 22' 57" W, a distance of 173.85 feet; thence S 41° 07' 26" W, a distance of 83.33 feet; thence S 33° 39' 54" W, a distance of 1014.22 feet; thence S 38° 13' 10" W, a distance of 254.01 feet; thence S 42° 13' 45" W, a distance of 379.40 feet; thence S 49° 52' 24" W, a distance of 459.94 feet; thence S 78° 56' 00" W, a distance of 182.58 feet; thence S 87° 40' 22" W, a distance of 913.61 feet, to the Point of Beginning. Containing

The above described parcel contains 143.59 acres, more or less.

And

A parcel of land located in a portion of Sections 6, 7, 8, 17, 18 and 19, Township 21 South, Range 64 West, 6th P.M., and a portion of Sections 12, 13, 23, 24, 25 and 26, Township 21 South, Range 65 West, 6th P.M., County of Pueblo, State of Colorado, being more particularly described as follows:

Beginning at a point on an existing fence line from which point the SE corner of Section 23, Township 21 South, Range 65 West, bears: S 76° 57' 25" E, a distance of 791.33 feet, being a ¾" rebar with a 3 ¼" aluminum cap, PLS 11550; thence along said fence line the following five (5) courses:

- 1) S 40° 47' 29" E, a distance of 78.75 feet;
 - 2) S 14° 24' 31" W, a distance of 398.22 feet;
 - 3) S 17° 55' 25" E, a distance of 499.73 feet;
 - 4) S 22° 42' 28" E, a distance of 795.32 feet;
 - 5) S 30° 43' 24" E, a distance of 85.69 feet, to a point 100 feet North of Comanche Water line; thence N 89°36' 55" E, parallel to said Water line a distance of 1333.32 feet, to a point on an existing fence line; thence along said fence line the following three (3) courses:
- 1) N 40° 28' 59" E, a distance of 390.65 feet;

1939418 ENV COVT 04/09/2013 02:56:18 PM
Page: 10 of 14 R 91.00 D 0.00 T 91.00
Gilbert Ortiz Clerk/Recorder, Pueblo County, Co



- 2) N 37° 15' 56" E, a distance of 1288.68 feet;
- 3) N 27° 16' 28" E, a distance of 1173.55 feet; thence departing said fence line N 27° 37' 33" E, a distance of 2098.43 feet to a fence corner; thence N 71° 48' 09" E, a distance of 553.20 feet, to the westerly right-of-way line of Burlington Northern, Atchison, Topeka and Santa Fe Rail Road; thence along said westerly right-of-way the following ten (10) courses:

- 1) on the arc of a curve to the Right, from which the radius point bears S 51° 39' 24" E, through a central angle of 02° 15' 00", whose radius is 2596.61 feet, an arc length of 101.97 feet;
- 2) N 40° 35' 36" E, a distance of 1713.48 feet;
- 3) N 00° 03' 32" W, a distance of 38.37 feet;
- 4) N 40° 35' 36" E, a distance of 1462.18 feet;
- 5) N 49° 24' 25" W, a distance of 25.00 feet;
- 6) on the arc of a curve to the Left, from which the radius point bears N 49° 24' 24" W, through a central angle of 00° 31' 09", whose radius is 7539.48 feet, an arc length of 68.31 feet;
- 7) on the arc of a curve to the Left, from which the radius point bears N 49° 56' 33" W, through a central angle of 00° 12' 51", whose radius is 7539.48 feet, an arc length of 28.18 feet;
- 8) on the arc of a curve to the Left, from which the radius point bears N 50° 09' 22" W, through a central angle of 17° 46' 47", whose radius is 5629.48 feet, an arc length of 1746.91 feet;
- 9) on the arc of a curve to the Left, from which the radius point bears N 67° 56' 09" W, through a central angle of 00° 45' 00", whose radius is 7539.48 feet, an arc length of 98.69 feet;
- 10) N 21° 18' 51" E, a distance of 260.57 feet, said point being the intersection of the westerly right-of-way of said rail road and an existing fence line if projected northwesterly on the east side of said rail road right-of-way; thence along said fence line the following ten (10) courses:

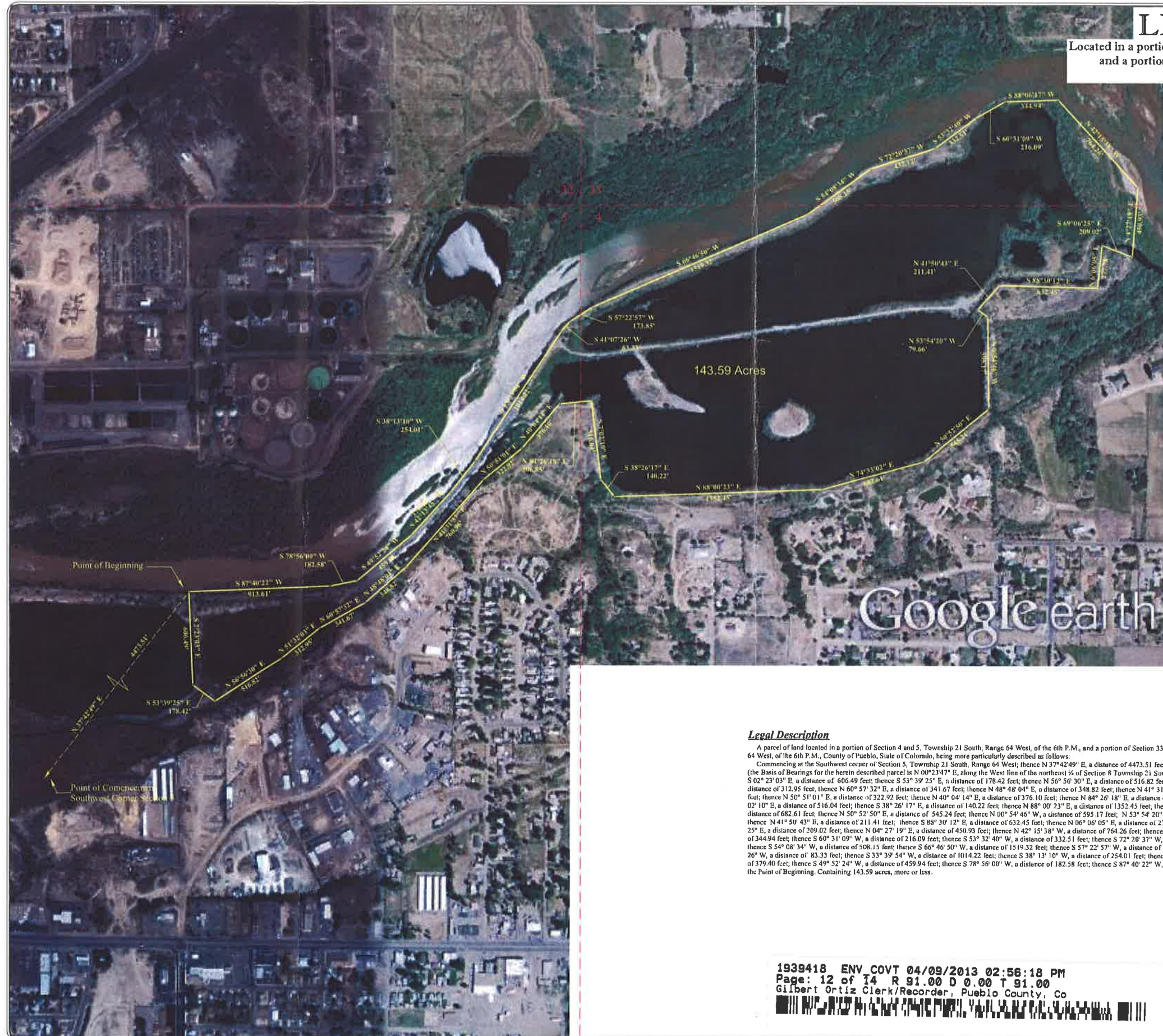
- 1) S 58° 00' 41" E, a distance of 1050.78 feet;
- 2) N 44° 05' 02" E, a distance of 318.41 feet;
- 3) N 10° 04' 41" W, a distance of 1136.46 feet;
- 4) N 40° 20' 25" E, a distance of 1068.33 feet;
- 5) S 69° 11' 17" E, a distance of 1331.93 feet;
- 6) S 88° 37' 12" E, a distance of 440.20 feet;
- 7) S 65° 57' 38" E, a distance of 1242.96 feet;
- 8) N 10° 47' 51" W, a distance of 1284.47 feet;
- 9) N 79° 36' 24" W, a distance of 205.51 feet;
- 10) N 01° 15' 20" E, a distance of 966.02 feet; thence departing said fence line N 37° 14' 25" W, a distance of for 1110.02 feet, to a power pole; thence N 21° 10' 16" W, a distance of 1832.81 feet to a power pole; thence N 24° 58' 24" W, a distance of 804.27 feet, to the westerly right-of-way line of the Burlington Northern, Atchison, Topeka and Santa Fe Rail Road; thence along said westerly right-of-way line the following fourteen (14) courses:

- 1) on the arc of a curve to the Left, from which the radius point bears N 79° 21' 08" W, through a central angle of 25° 49' 22", whose radius is 1562.87 feet, an arc length of 704.37 feet;
- 2) N 74° 49' 31" E, a distance of 10.00 feet;
- 3) on the arc of a curve to the Left, from which the radius point bears S 74° 49' 30" W, through a central angle of 02° 37' 56", whose radius is 2116.76 feet, an arc length of 97.25 feet;
- 4) N 17° 48' 26" W, a distance of 608.97 feet;
- 5) N 72° 11' 34" E, a distance of 10.00 feet;
- 6) N 17° 48' 26" W, a distance of 50.00 feet;
- 7) on the arc of a curve to the Right, through a central angle of 02° 41' 15", whose radius is 2181.97 feet, an arc length of 102.35 feet;
- 8) on the arc of a curve to the Right, from which the radius point bears N 74° 52' 50" E, through a central angle of 19° 24' 43", whose radius is 1649.21 feet, an arc length of 558.76 feet;
- 9) N 85° 42' 27" W, a distance of 10.00 feet;



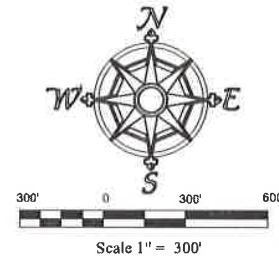
- 10) on the arc of a curve to the Right, from which the radius point bears S 85° 42' 27" E, through a central angle of 13° 52' 21", whose radius is 1659.21 feet, an arc length of 401.73 feet;
- 11) on the arc of a curve to the Right, from which the radius point bears S 71° 50' 06" E, through a central angle of 02° 41' 15", whose radius is 2191.97 feet, an arc length of 102.82 feet;
- 12) N 19° 58' 47" E, a distance of 169.85 feet;
- 13) S 74° 21' 09" E, a distance of 10.00 feet;
- 14) N 15° 39' 00" E, a distance of 487.35 feet; thence departing said westerly right-of-way N 88° 42' 21" W, a distance of 429.57 feet, to the southerly right-of-way of Northern Avenue; thence along said southerly right-of-way line the following thirteen (13) courses:
 - 1) S 77° 52' 44" W, a distance of 43.49 feet;
 - 2) S 81° 57' 44" W, a distance of 59.28 feet;
 - 3) S 80° 34' 14" W, a distance of 57.48 feet;
 - 4) S 79° 52' 14" W, a distance of 27.22 feet;
 - 5) S 75° 38' 44" W, a distance of 80.11 feet;
 - 6) S 69° 37' 14" W, a distance of 83.20 feet;
 - 7) S 65° 01' 14" W, a distance of 696.34 feet;
 - 8) S 66° 13' 14" W, a distance of 102.16 feet;
 - 9) S 69° 10' 14" W, a distance of 104.42 feet;
 - 10) S 74° 40' 14" W, a distance of 102.48 feet;
 - 11) S 73° 55' 14" W, a distance of 95.07 feet;
 - 12) S 89° 33' 09" W, a distance of 2544.24 feet;
 - 13) S 89° 32' 50" W, a distance of 1242.04 feet, to the easterly right-of-way line of the Denver and Rio Grande Rail Road; thence S 00° 27' 12" E, along said easterly right-of-way, a distance of 3937.29 feet; thence departing said easterly right-of-way N 89° 44' 27" W, a distance of 100.00 feet, to the northeast corner of Block 25, Lake Minnequa Addition, Amended; thence N 89° 44' 27" W, along the north line of said Block 25, a distance of 368.55 feet, to a point on the Easterly right-of-way line of Colorado Interstate No. 25; thence along said easterly right-of-way line the following thirteen (13) courses:
 - 1) S 12° 51' 28" W, a distance of 201.04 feet;
 - 2) on the arc of a curve to the Left, from which the radius point bears S 73° 01' 19" E, through a central angle of 06° 52' 01", whose radius is 1402.50 feet, an arc length of 168.09 feet;
 - 3) S 06° 21' 55" W, a distance of 153.40 feet;
 - 4) S 60° 53' 05" E, a distance of 97.12 feet;
 - 5) on the arc of a curve to the Left, from which the radius point bears S 65° 22' 31" E, through a central angle of 11° 34' 24", whose radius is 2367.00 feet, an arc length of 478.11 feet;
 - 6) S 00° 15' 33" W, a distance of 239.50 feet;
 - 7) S 06° 03' 55" W, a distance of 361.10 feet;
 - 8) S 00° 51' 04" E, a distance of 684.37 feet;
 - 9) S 00° 05' 05" E, a distance of 134.70 feet;
 - 10) on the arc of a curve to the Left, from which the radius point bears S 86° 25' 03" E, through a central angle of 04° 04' 04", whose radius of 3880.00 feet, an arc length of 275.46 feet;
 - 11) S 02° 33' 50" W, a distance of 864.99 feet;
 - 12) S 24° 45' 55" W, a distance of 454.47 feet;
 - 13) S 39° 04' 52" W, a distance of 616.40 feet; thence departing said easterly right-of-way line S 25° 00' 31" W, a distance of 8383.90 feet, to the Point of Beginning.

Containing 2050.41 acres, more or less.



LEGAL DESCRIPTION

Located in a portion of Sections 4 and 5, Township 21 South, Range 64 West, 6th P.M. and a portion of Sections 33, Township 20 South, Range 64 West, 6th P.M. County of Pueblo, State of Colorado



BASIS OF BEARINGS: N 00°23'47" E, (per GPS observation) along the East line of the Northeast 1/4, Section 7, Township 21 South, Range 64 West, of the 6th P.M. Monumented with a 3/4" rebar, and 3 1/4" aluminum cap at each end.

Legal Description

A parcel of land located in a portion of Section 4 and 5, Township 21 South, Range 64 West, of the 6th P.M., and a portion of Section 33, Township 20 South, Range 64 West, of the 6th P.M., County of Pueblo, State of Colorado, being more particularly described as follows:
 Commencing at the Southwest corner of Section 5, Township 21 South, Range 64 West; thence N 37°42'49" E, a distance of 4473.51 feet, to the Point of Beginning (the Basis of Bearings for the herein described parcel is N 00°23'47" E, along the West line of the northeast 1/4 of Section 8 Township 21 South, Range 64 West); thence S 02°23'03" E, a distance of 606.49 feet; thence S 53°39'25" E, a distance of 178.42 feet; thence N 56°56'30" E, a distance of 516.82 feet; thence N 51°32'03" E, a distance of 312.95 feet; thence N 60°57'32" E, a distance of 341.67 feet; thence N 48°48'04" E, a distance of 348.82 feet; thence N 41°31'37" E, a distance of 760.96 feet; thence N 50°51'01" E, a distance of 322.92 feet; thence N 40°04'14" E, a distance of 376.10 feet; thence N 84°26'18" E, a distance of 206.85 feet; thence S 07°02'10" E, a distance of 516.04 feet; thence S 38°26'17" E, a distance of 140.22 feet; thence N 88°00'23" E, a distance of 1352.45 feet; thence N 74°33'02" E, a distance of 682.61 feet; thence N 50°52'50" E, a distance of 545.24 feet; thence N 00°54'46" W, a distance of 595.17 feet; N 53°54'20" W, a distance of 79.66 feet; thence N 41°50'43" E, a distance of 211.41 feet; thence S 88°30'12" E, a distance of 632.45 feet; thence N 06°06'05" E, a distance of 277.78 feet; thence S 69°06'25" E, a distance of 209.02 feet; thence N 04°27'19" E, a distance of 450.93 feet; thence N 42°15'38" W, a distance of 764.26 feet; thence S 88°06'17" W, a distance of 344.94 feet; thence S 60°31'09" W, a distance of 316.09 feet; thence S 53°32'40" W, a distance of 332.51 feet; thence S 72°20'37" W, a distance of 432.12 feet; thence S 54°08'34" W, a distance of 508.15 feet; thence S 66°46'50" W, a distance of 1519.32 feet; thence S 57°22'57" W, a distance of 173.85 feet; thence S 41°07'26" W, a distance of 83.33 feet; thence S 33°39'54" W, a distance of 1014.22 feet; thence S 38°13'10" W, a distance of 254.01 feet; thence S 42°13'45" W, a distance of 379.40 feet; thence S 49°52'24" W, a distance of 459.94 feet; thence S 78°56'00" W, a distance of 182.58 feet; thence S 87°40'22" W, a distance of 913.61 feet, to the Point of Beginning. Containing 143.59 acres, more or less.

1939418 ENV COVT 04/09/2013 02:56:18 PM
 Page: 12 of 14 R 91.00 D 0.00 T 91.00
 Gilbert Ortiz Clerk/Recorder, Pueblo County, Co



Date: 4-11-2011	Revision Description	By: wth
Legal Description Evraz Steel Project No. S12-11		
Of Colorado, Inc. Professional Land Surveyors 813, D Duquesne Blvd. Pueblo, CO 81002 Phone: 719-542-9169 Fax: 719-542-9183		

LEGAL DESCRIPTION

A portion of land located in the East Half and Northwest Quarter of Section 6, Township 21 South, Range 64 West of the 6th P.M. in Pueblo County, Colorado



LEGAL DESCRIPTION:

A tract of land located in the East Half and the Northwest Quarter of Section 6, Township 21 South, Range 64 West of the 6th P.M. in Pueblo County, Colorado more particularly described as follows:

Commencing from the Center of said Section 6, also being the Point of Beginning; Thence 40 feet ± North along the North-South Center line of said Section 6 and the East line of a parcel of land recorded at Reception No. 1893835 of the Pueblo County records, to a point on the North line of said parcel; Thence N60°05'51"W, a distance of 191 feet ±, along said North line to a point being the Northwest corner of said parcel; Thence S29°59'54"W, a distance of 30 feet ± along the West line of said parcel to a point being the Northeast Corner of Bullen's Subdivision; Thence N61°00'06"W, a distance of 1879 feet ± to a point on the centerline of Stanton Street; Thence continuing North along said Stanton St., a distance of 178 feet ± to the South bank of the Arkansas River; Thence Easterly along said South bank of the Arkansas River to a point on the West line of the BNSF Railroad right of way; Thence S2°36'47"E, a distance of 676.9 feet ± along said West right of way line; Thence continuing along said West right of way on a curve to the right with a radius of 1854 feet ± and a length of 894.52 feet ±; Thence continuing along West right of way S14°31'48"W, a distance of 543.2 feet ± to a point on the North right of way line of U.S. HWY 50; Thence along said U.S. HWY 50 the following 12 courses: Thence N76°35'58"W, a distance of 126 feet ±; Thence on a curve to the right with a radius of 2740 feet ± and an arc length of 341.14 feet ±; Thence N68°54'00"W, a distance of 358.3 feet; Thence N21°13'01"E, a distance of 110.0 feet; Thence N69°03'37"W, a distance of 279.9 feet; Thence S21°03'33"W, a distance of 25.0 feet; Thence N68°55'47"W, a distance of 89.2 feet; Thence S22°21'21"W, a distance of 45.0 feet; Thence S20°52'02"W, a distance of 10.0 feet; Thence N68°39'08"W, a distance of 139.7 feet; Thence S21°18'36"W, a distance of 10.0 feet; Thence N73°45'30"W, a distance of 13.5 feet ± to a point on the East line of a parcel recorded in Book 2769 Pg. 913 of the Pueblo County records; Thence North, a distance of 357.4 feet ± along said parcel to the Northeast corner of said parcel; Thence West, a distance of 141 feet ± along said North line of said parcel to a point, being the East line of a parcel recorded at Reception No. 1893835 of the Pueblo County records; Thence North along said East line a distance of 191 feet ± to the Northeast corner of said parcel; Thence N85°57'W, a distance of 455.0 feet ± to a point on the North-South Centerline of said Section 6; Thence North along said centerline, a distance of 51 feet ± to the Center of Section 6 being the Point of Beginning.

The above described parcel contains 73± Acres

DATE	REVISION DESCRIPTION	BY
	Legal Description	
	EVRAZ	
	Project Number: S12-11	
	PRELIMINARY	

Alta Surveys
Of Colorado, Inc. Professional Land Surveyors
813, B Desertflower Blvd. Pueblo, CO 81001
Phone: 719-542-9169 Fax: 719-542-9183

SCALE: 1" = 200'

Found CDOT Monuments 3-1/4" Aluminum Cap

1939418 ENV COVT 04/09/2013 02:56:18 PM
Page: 13 of 14 R 91.00 D 0.00 T 91.00
Gilbert Ortiz Clerk/Recorder, Pueblo County, Co

1939418 ENV COVT 04/09/2013 02:56:18 PM
 Page: 14 of 14 R 91 00 D 0 00 T 91 00
 Gilbert Ortiz Clerk/Recorder, Pueblo County, CO

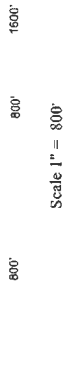


LEGAL DESCRIPTION

Located in a portion of Sections 6, 7, 8, 17, 18 and 19, Township 21 South, Range 64 West, 6th P.M., and a portion of Sections 12, 13, 23, 24, 25 and 26, Township 21 South, Range 65 West, 6th P.M., County of Pueblo, State of Colorado

Line Table

LINE	BEARING	HORIZ DIST	CURVE	DELTA	RADIUS	ARC	BEARING	HORIZ DIST
L1	S40°47'29"E	78.75'	C1	0°31'09"	7539.48'	68.31'	N40°20'01"E	68.31'
L2	S14°24'31"W	398.22'	C2	2°15'00"	2596.61'	101.96'	N39°28'06"E	101.96'
L3	S17°55'25"E	489.73'	C3	0°12'51"	7539.48'	28.18'	N39°57'01"E	28.18'
L4	S30°45'24"E	85.69'	C4	17°46'47"	5629.48'	1746.91'	N30°57'14"E	1739.91'
L5	N40°28'59"E	390.65'	C5	0°45'00"	7539.48'	98.69'	N21°41'21"E	98.69'
L6	N42°52'42"E	53.37'	C6	25°49'22"	1502.87'	704.37'	N21°15'48"W	698.42'
L7	N49°03'31"W	33.37'	C7	2°41'15"	218.97'	102.35'	N16°57'48"W	102.35'
L8	N49°26'25"W	25.00'	C8	7°24'15"	1649.21'	102.35'	N16°57'48"W	102.35'
L9	N21°18'51"E	260.57'	C9	19°24'43"	1649.21'	558.76'	N8°27'48"W	558.09'
L10	N44°05'02"E	318.41'	C10	13°55'21"	1659.21'	401.73'	N11°13'40"E	400.75'
L11	S88°57'12"E	440.20'	C11	2°41'15"	2191.97'	102.82'	N19°30'31"E	102.81'
L12	N79°36'24"W	305.51'	C12	6°52'01"	1402.50'	168.09'	S13°32'41"W	167.99'
L13	N78°49'31"E	10.00'	C13	11°34'24"	2367.00'	478.11'	S18°50'17"W	477.30'
L14	N68°56'44"E	608.97'	C14	4°04'04"	3680.00'	275.46'	S13°55'55"W	275.40'
L15	N17°41'54"E	50.00'						
L16	N17°48'26"W	50.00'						
L17	N85°48'27"W	10.00'						
L18	N19°58'47"E	169.85'						
L19	S74°21'09"E	10.00'						
L20	N15°59'00"E	487.35'						
L21	N88°42'21"W	429.57'						
L22	S77°52'44"W	43.99'						
L23	S77°52'44"W	43.99'						
L24	S80°34'14"W	57.48'						
L25	S79°52'14"W	27.22'						
L26	S75°26'44"W	80.11'						
L27	S69°43'14"W	83.20'						
L28	S66°13'14"W	102.16'						
L29	S69°10'14"W	104.42'						
L30	S74°40'14"W	102.48'						
L31	S74°40'14"W	102.48'						
L32	N49°54'27"W	10.00'						
L33	N89°44'27"W	368.55'						
L34	S12°51'28"W	201.04'						
L35	S8°21'55"W	153.40'						
L36	S60°53'05"E	97.12'						
L37	S09°15'33"W	239.50'						
L38	S67°05'55"W	361.10'						
L39	S67°05'55"W	361.10'						
L40	S74°40'14"W	124.70'						
L41	S74°40'14"W	124.70'						
L42	S39°04'52"W	616.40'						



LEGEND

- ▲ FOUND 3/4" REBAR, WITH 3 1/4" ALUMINUM CAP, PLS 11550
- ◆ FOUND T-RAIL 12" ABOVE GROUND, PROPERLY MARKED

BASIS OF BEARINGS: N 00°23'47" E, (per GPS observation) along the East line of the Northeast 1/4, Section 7, Township 21 South, Range 64 West, of the 6th P.M. Monumented as shown hereon

Curve Table

LINE	BEARING	HORIZ DIST	CURVE	DELTA	RADIUS	ARC	BEARING	HORIZ DIST
L1	S40°47'29"E	78.75'	C1	0°31'09"	7539.48'	68.31'	N40°20'01"E	68.31'
L2	S14°24'31"W	398.22'	C2	2°15'00"	2596.61'	101.96'	N39°28'06"E	101.96'
L3	S17°55'25"E	489.73'	C3	0°12'51"	7539.48'	28.18'	N39°57'01"E	28.18'
L4	S30°45'24"E	85.69'	C4	17°46'47"	5629.48'	1746.91'	N30°57'14"E	1739.91'
L5	N40°28'59"E	390.65'	C5	0°45'00"	7539.48'	98.69'	N21°41'21"E	98.69'
L6	N42°52'42"E	53.37'	C6	25°49'22"	1502.87'	704.37'	N21°15'48"W	698.42'
L7	N49°03'31"W	33.37'	C7	2°41'15"	218.97'	102.35'	N16°57'48"W	102.35'
L8	N49°26'25"W	25.00'	C8	7°24'15"	1649.21'	102.35'	N16°57'48"W	102.35'
L9	N21°18'51"E	260.57'	C9	19°24'43"	1649.21'	558.76'	N8°27'48"W	558.09'
L10	N44°05'02"E	318.41'	C10	13°55'21"	1659.21'	401.73'	N11°13'40"E	400.75'
L11	S88°57'12"E	440.20'	C11	2°41'15"	2191.97'	102.82'	N19°30'31"E	102.81'
L12	N79°36'24"W	305.51'	C12	6°52'01"	1402.50'	168.09'	S13°32'41"W	167.99'
L13	N78°49'31"E	10.00'	C13	11°34'24"	2367.00'	478.11'	S18°50'17"W	477.30'
L14	N68°56'44"E	608.97'	C14	4°04'04"	3680.00'	275.46'	S13°55'55"W	275.40'

Legal Description

A parcel of land located in a portion of Sections 6, 7, 8, 17, 18 and 19, Township 21 South, Range 64 West, 6th P.M., and Sections 12, 13, 23, 24, 25 and 26, Township 21 South, Range 65 West, 6th P.M., County of Pueblo, State of Colorado, containing more particularly described as follows:
 Beginning at a point on an existing fence line from which point the S/E corner of Section 23, Township 21 South, Range 65 West, bears S 70° 57' 25" E, a distance of 791.33 feet, being a 3/4" rebar with a 3 1/4" aluminum cap, PLS 11550; thence along the following bearings and distances:
 1) S 15° 46' 47" 29" E, a distance of 78.75 feet;
 2) S 17° 55' 25" E, a distance of 489.73 feet;
 3) N 00° 03' 37" W, a distance of 78.37 feet;
 4) N 49° 35' 30" E, a distance of 162.18 feet;
 5) S 17° 55' 25" E, a distance of 489.73 feet;
 6) on the arc of a curve to the Left, from which the radius point bears N 49° 34' 24" W, through a central angle of 00° 31' 09", whose radius is 2596.61 feet, an arc length of 68.31 feet;
 7) on the arc of a curve to the Left, from which the radius point bears N 49° 56' 23" W, through a central angle of 00° 12' 51", whose radius is 7539.48 feet, an arc length of 28.18 feet;
 8) on the arc of a curve to the Left, from which the radius point bears N 50° 09' 22" W, through a central angle of 17° 46' 47", whose radius is 5629.48 feet, an arc length of 1746.91 feet;
 9) on the arc of a curve to the Left, from which the radius point bears N 67° 56' 00" W, through a central angle of 00° 45' 00", whose radius is 7539.48 feet, an arc length of 98.69 feet;
 10) N 21° 18' 51" E, a distance of 260.57 feet, and point being the intersection of the westerly right-of-way of said rail road and an
 northwesterly on the east side of said rail road right-of-way; thence along said
 1) N 44° 05' 02" E, a distance of 169.85 feet;
 2) N 09° 44' 27" W, a distance of 368.55 feet;
 3) N 09° 44' 27" W, a distance of 368.55 feet;
 4) S 09° 15' 33" E, a distance of 239.50 feet;
 5) S 09° 15' 33" E, a distance of 239.50 feet;
 6) S 88° 57' 12" E, a distance of 440.20 feet;
 7) S 09° 57' 30" E, a distance of 124.70 feet;
 8) S 09° 57' 30" E, a distance of 124.70 feet;
 9) N 79° 36' 24" W, a distance of 305.51 feet;
 10) N 0° 03' 37" E, a distance of 78.37 feet; thence departing said fence line N 37° 14' 25" W, a distance of 110.02 feet to a power pole; thence N 21° 10' 16" W, a distance of 183.31 feet to a power pole; thence N 24° 58' 24" W, a distance of 124.70 feet to a power pole; thence S 18° 50' 17" W, a distance of 477.30 feet to a power pole; thence S 13° 32' 41" W, a distance of 167.99 feet to a power pole; thence N 11° 13' 40" E, a distance of 400.75 feet to a power pole; thence N 8° 27' 48" W, a distance of 558.09 feet to a power pole; thence N 16° 57' 48" W, a distance of 102.35 feet to a power pole; thence N 21° 15' 48" W, a distance of 698.42 feet to a power pole; thence N 21° 41' 21" E, a distance of 98.69 feet to a power pole; thence N 30° 57' 14" E, a distance of 1739.91 feet to a power pole; thence N 39° 28' 06" E, a distance of 101.96 feet to a power pole; thence N 40° 20' 01" E, a distance of 28.18 feet to a power pole; thence N 39° 57' 01" E, a distance of 28.18 feet to a power pole; 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